

Solicitation No.: 25-0196-RFP-CCNA

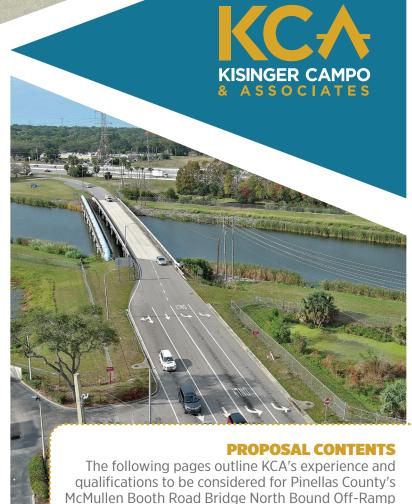
MCMULLEN BOOTH ROAD BRIDGE NORTH BOUND OFF-RAMP OVER LAKE TARPON OUTFALL CANAL

CONTACT INFORMATION:

Kisinger Campo & Associates, Corp.

Erin Lawson, PE, Project Manager **Local Office:** 201 North Franklin Street, Suite 900, Tampa, FL 33602 o. 813.871.5331| f. 813.871.5135 ELawson@kcaeng.com www.kisingercampo.com

FEBRUARY 27, 2025



Over Lake Tarpon Outfall Canal project.



February 27, 2025

Pinellas County Board of County Commissioners (BOCC) 400 S. Ft. Harrison Avenue, Clearwater, FL 33756 Annex Building – 6th Floor

Attn: Alex Meloy, CPPB, NIGP-CPP, Lead Procurement Analyst

Re: McMullen Booth Road Bridge North Bound Off-Ramp Over Lake Tarpon Outfall Canal | Professional Engineering Services | Solicitation No.: 25-0196-RFP-CCNA Kisinger Campo & Associates, Corp. (KCA) 201 North Franklin Street, Suite 900 Tampa, FL 33602

> Erin Lawson, PE Dedicated Project Manager

ELawson@kaeng.com 0: 813.871.5331

Kisinger Campo & Associates, Corp. (KCA), your local, full-service engineering firm, understands the importance of selecting the most qualified engineering firm for the replacement of the McMullen Booth Road (CR 611) NB Off-Ramp bridge over Lake Tarpon Outfall Canal. Our firm is well-positioned to deliver this critical infrastructure project efficiently and effectively, ensuring compliance with all Pinellas County and Florida Department of Transportation (FDOT) standards.

The KCA team, led by **Erin Lawson, PE**, is committed to delivering this project efficiently, maintaining regular communication with the County, and providing ongoing support through design, construction, and stakeholder coordination.

With our extensive experience in bridge design and transportation infrastructure projects, we are confident in our ability to deliver construction plans for a replacement structure that enhances safety, durability, and accessibility. Our team of professional engineers has successfully completed similar projects, demonstrating our technical expertise and commitment to delivering high-quality solutions. We understand the importance of meeting schedule and budget constraints and have the necessary resources to ensure design completion of this project in order for construction to be complete by January 2030.

OUR FIRM BRINGS:

- An experienced Project Manager (PM) with knowledge of Pinellas County preferences.
- A team of skilled professionals with deep expertise in structural, roadway, and drainage engineering.
- A proven track record of delivering infrastructure projects of similar scope and complexity for the County and other local municipalities.
- A robust project management framework to ensure ontime delivery and adherence to budgetary constraints.

We are eager to collaborate with Pinellas County and continue a long history of delivering successful projects for Pinellas County. Our team, led by Ms. Lawson will provide a sustainable and long-lasting bridge solution that meets current and future transportation and multi-modal demands. Our firm is prepared to commence work immediately upon contract award and looks forward to the opportunity to contribute to the success of this vital project.

KCA pledges to use the knowledge gained from our past Pinellas County experience along with the talents and dedication of our current staff to continue providing unmatched service to Pinellas County's satisfaction. If you have any questions or require additional information, please contact our PM, Ms. Lawson, at 813.871.5331 or elawson@kcaeng.com. Thank you for your consideration.

Sincerely,

Kisinger Campo & Associates, Corp.

Guiller Madriz, PE, Vice President/Principal-in-Charge

- Project and corridor familiarity having performed the Phase I and Phase II Bridge Replacement Study and Report, design of Pinellas Trail North Gap Bridge over Lake Tarpon Outfall Canal (in construction), and selected designer for the County's East Lake Road over Brooker Creek Bridge Widenings (begin design this year).
- A strategic approach to coordination with the adjacent projects, including the East Lake Road Corridor Project Development & Environment (PD&E) Study.

WHY KCA TEAM

The KCA team is a local team composed of the high-quality professionals who live in the area, use this corridor, and are intimately familiar with the work being conducted adjacently. Some highlights that set us apart include:

- Experienced PM and Deputy PM
- Thorough understanding of Pinellas County's and general public's requirements for this contract
- Experienced technical personnel in all disciplines, with proven track records
- Strong internal Quality Assurance/Quality Control (QA/QC) process
- Necessary resources, including equipment, facilities, and the support of KCA corporate officers
- Committed to the success of the project

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SPECIFIC SERVICES TO BE OFFERED

 K^{0}

CA is a full-service transportation engineering firm with highly skilled and experienced staff. Specific responsibilities of each team member for this project are included below:

SERVICES OFFERED BY FIRMS

Prime



KCA

- Project management
- Quality assurance/quality control (QA/QC)
- Structures analysis and plans
- Roadway analysis and plans
- Drainage analysis and plans
- Signing and pavement marking (S&PM) analysis and plans
- Lighting analysis and plans
- Pavement design
- Construction cost estimates
- Technical specifications and special provisions
- Environmental evaluation and permitting
- Public involvement
- Post design

Subconsultants



Cumbey & Fair, Inc. (C&F)

■ Subsurface Utility Engineering (SUE)



Diversified Professional Services, Corp. (DPS)

■ Geotechnical/Contamination



Independence Acquisition & Appraisal, LLC (IAA)

■ Right-of-Way (R/W) Acquisition



Janus Research, Inc. (Janus)

Cultural resources



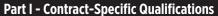
Tierra, Inc. (Tierra)

■ Geotechnical/Contamination

01. Standard Form (SF) 330 Parts I and II

s requested in Pinellas County's RFP, KCA has included our SF 330 in this section.

ARCHITECT - ENGINEER QUALIFICATIONS





A. Contract Information

1. TITLE AND LOCATION (City and State)

McMullen Booth Road Bridge North Bound Off-Ramp Over Lake Tarpon Outfall Canal, Pinellas County

2. PUBLIC NOTICE DATE 3. SOLICITATION OR PROJECT NUMBER

January 27,2025 25-0196-RFP-CCNA

B. Architect-Engineer Point of Contact

4. NAME AND TITLE

Erin Lawson, PE, Project Manager

5. NAME OF FIRM

Kisinger Campo & Associates, Corp. (KCA)

6. TELEPHONE NUMBER 7. FAX NUMBER 8. E-MAIL ADDRESS

813.871.5331 813.871.5135 elwason@kcaeng.com

C. Proposed Team

	C. Proposed Team (Complete this section for the prime contractor and all key subcontractors.)							
	PRIME	J-V PARTNER	SUBCONTRACTOR	9. FIRM NAME	10. ADDRESS	11. ROLE IN THIS CONTRACT		
a.				Kisinger Campo & Associates, Corp. (KCA) CHECK IF BRANCH OFFICE	201 N. Franklin Street, Suite 900 Tampa, FL 33602	Project Management; QA/QC; Roadway Design; Stormwater Design; Structures Design; Traffic Analysis and Engineering; ITS; Environmental/Permitting; Public Involvement		
b.				KCA CHECK IF BRANCH OFFICE	111 N. Magnolia Avenue, Suite 1050 Orlando, FL 32801	Traffic Analysis and Engineering; ITS		
С.				Cumbey & Fair, Inc. (C&F) [SBE] CHECK IF BRANCH OFFICE	2463 Enterprise Road,Clearwater, FL 33763	SUE		
d.				Diversified Professional Service, Corp. (DPS)	27915 Johnston Road, Dade City, FL 33523	Geotechnical/Contamination		
е.				Independence Acquisition & Appraisal, LLC (IAA)	11030 US Highway 301, Thonotosassa, FL 33592	Right-of-Way Acquisition		
f.				CHECK IF BRANCH OFFICE Janus Research, Inc. (Janus) CHECK IF BRANCH OFFICE	1107 N Ward Street, Tampa, FL 33607	Cultural Resources		
g.				Tierra, Inc. (Tierra) CHECK IF BRANCH OFFICE	7351 Temple Terrace Highway, Tampa, FL 33637	Geotechnical/Contamination		
h.				CHECK IF BRANCH OFFICE				
i.				CHECK IF BRANCH OFFICE				
j.				CHECK IF BRANCH OFFICE				

Pinellas County PM

PRINCIPAL-IN-CHARGE

PROJECT MANAGER

QA/QC

Guillermo Madriz, PE (27)*

Erin Lawson, PE (24)*

Richard Harrison, PE (42)*

DEPUTY PROJECT MANAGER

Stephanie Boyd, PE (10)*

ROADWAY ANALYSIS AND PLANS

Deborah Hernandez, PE (19)*

Dean Grumbach, PE (9)* Jessica Alexandre, EI (2)

QC: John Mazeres, PE (38)

STRUCTURES ANALYSIS AND PLANS

Stephanie Bovd. PE (10)*

Austin Black, PE (11)* Kip Laskaris, PE (26)

QC: Thomas Lovett, PE (46)

S&PM | LIGHTING ANALYSIS AND PLANS

Dr. Fathy Abdalla, PE, PTOE (29)*

Nick Russin, PE, RSP1 (5)* Craig Singer, PE, PTOE, RSP1 (5)

QC: Josh Robinson, PE (37)

ENVIRONMENTAL EVALUATION/PERMITTING

Tom Pride (35)*

Ashley Gorham (14)*

QC: Robert Whitman, Jr. (43)

PUBLIC INVOLVEMENT

Nicole Selly, ENV SP (19)*

Diana Albarracin (21)*

QC: Dick Combs, CEP (45)

DRAINAGE/HYDRAULICS ANALYSIS AND PLANS

Tara Spieler, PE (29)*

Gabe Garcia, PE (6)* Tiffany Buchanan, EI (6)

QC: Ali Tayebnejad, PE (38)

CONSTRUCTABILITY/ BIDABILITY

Kellie Loper, PE (22)*

Ken Spillett (38)

QC: Dewey Martin, PE (41)

CULTURAL RESOURCES (JANUS)

Kenneth Hardin (45)*

Kathleen Hoffman (44)* Amy Streelman (27)*

RIGHT-OF-WAY AQUISITION (IAA)

Kelly Joslin (26)*

Rick Joslin (35)* Angelia Stevenson (27)*

SUE (C&F)

Pat McCormack, PSM (29)*

GEOTECHNICAL/ CONTAMINATION (TIERRA/DPS)

Joseph Antinori, PE (18)*

Dr. Marc Novak, PE (18)*
Michael Bair, ASP (28)*
Chris Garth, LEP (16)
Sandra Polanis (20)*[DPS]

SUBCONSULTANTS (Florida MBE | • Pinellas County SBE)

- Cumbey & Fair, Inc. (C&F)
- Diversified Professional Services, Inc. (DPS)■●
- Independence Acquisition & Appraisal, LLC (IAA) ■●
- Janus Research, Inc. (Janus)
- Tierra, Inc. (Tierra)

(xx) years of experience

Denotes EOR/discipline lead

*Key personnel resumes included in Part E of

Standard Form 330

	E. R	esumes of Key P (Complete o	ersonnel Proposed for this Connection E for each key person.)	ontract	KISINGER CAMPO
12. NAME 13. ROLE IN T			TRACT	14. YEARS EXPERIENCE	
		Project Manager		a. TOTAL	b. WITH CURRENT FIRM
				24	16
15. FIRM	NAME AND LOCATION (City and State)				
	er Campo & Associates, Corp., Tampa, FL				
16. EDUC	ATION (Degree and Specialization)		17. CURRENT AND PROFESSIONAL REGIST	RATION (State and Discipline)	
BSCE, G	Georgia Institute of Technology		Professional Engineer: Florida #7094	3	
Ms. Law projects marking	R PROFESSIONAL QUALIFICATIONS (<i>Publications</i> , son has 24 years of experience in transporta so. Her expertise includes project managemen g (S&PM) plans, Americans with Disabilities A for Pinellas County Transportation Engineeri	tion engineering and it, roadway design, ma Act (ADA) compliance	design for interstate interchanges, loca aintenance of traffic (MOT), traffic conti , and approach alternatives services. M	rol plans (TCP), post-design, si s. Lawson has worked for KCA	gning and pavement
		19.	RELEVANT PROJECTS		
	(1) TITLE AND LOCATION (City and State)			(2) YEAR CON Professional Services	MPLETED Construction
	62nd Avenue Bridge Rehabilitation, City o	of St. Petersburg, FL		Ongoing	Construction
	(3) BRIEF DESCRIPTION (Brief scope, size, cost, et	Check if project	t performed with current firm		
a. Roadway Engineer. This project consists of the replacement of access to the Bayway Isles community in St. Petersburg, Florida deteriorated at an accelerated rate compared to conventional puthis bridge to be a part of its Local Agency Program. KCA has preplacement over navigable waters and in an environmentally sees: \$494,226			The 40-year-old bridge was originally on stressed beam bridges. As a result, the formed all structures, roadway, and MC	constructed with hollow core sl City of St. Petersburg along w IT design for this project. Due t d permitting efforts were also o	ab units, which have ith FDOT has selected o the superstructure completed in-house.
	(1) TITLE AND LOCATION (City and State)			(2) YEAR CON Professional Services	4PLETED Construction
	Venetian Boulevard Bridge Replacement, City of St. Petersburg, FL			2024	Ongoing
b.	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND	Check if project performed with current firm			
υ.	Roadway Engineer. KCA was selected under Replacement Project in Shore Acres. The ex and drainage reconstruction, seawall repairs incorporated public feedback to minimize of 2024, with construction starting in February	quiring structural design, perm nd vehicular bypass for constru	itting, roadway ction detours and		
	(1) TITLE AND LOCATION (City and State)			(2) YEAR CON Professional Services	MPLETED Construction
	Old Coachman over Alligator Creek - Bridg	ge No. 154252, Pinell	as County, FL	Professional services	N/A
C.	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Roadway Engineer for Post Design. Under our Pinellas County Miscellaneous Services contract, KCA was tasked to develop a multi-phased preliminary engineering report (PER) for Old Coachman Road over Alligator Creek to determine the condition of the existing bridge and adjacent structures and propose alternatives for design of a bridge replacement and roadway improvements. The bridge is functionally obsolete and has exceeded its design life. It also exhibit considerable deterioration. The initial bridge replacement study (Phase I) investigated whether these structures within an approximate 10-year period should be replaced, rehabilitated, or repaired. The Phase II replacement study, dated July 2012, addressed preliminary geometry, roadway, and structural design to meet the hydraulic needs of the site, including associated permitting, environmental, and construction considerations. Phase IIB included updated hydraulic modelling of the site and surrounding area based on the most recent City of Clearwater data. KCA proceeded with design of the replacement bridge to raise the profile, remove adjacent aging drainage structures, and improve safety. The hydraulic study and environmental permitting of the complex system was completed to minimize impact on the surrounding natural resources. The final design includes a three-span Florida Slab Beam superstructure for function and rapid construction to minimize road closure during construction.				
	(1) TITLE AND LOCATION (City and State)			(2) YEAR CON Professional Services	
	Westshore Interchange, Phased D/B, FDO	T District Seven, Tam	pa, FL	Ongoing	Construction N/A
	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND	SPECIFIC ROLE		Check if p	roject performed with current firm
d.	Senior Roadway Engineer - Team Lead. The Westshore Interchange and the I-275 and Send regional connectivity with general-purpenhancing the sidewalk and shared use path transit corridor within the I-275 median; and	R 60 corridors to delive cose lanes and new ex h network; inclusion o	ver congestion relief by improving acce opress lanes and direct connect ramps; f aesthetic treatments to mitigate cons	ssibility, mobility, and travel tin improve local street connectivi truction and enhance the comi	nes. It will add capacity ty and mobility by munity; preserve future

Continuing Services Contract for Design of FDOT LAP Projects | RFQ: 24-012 AUTHORIZED FOR LOCAL REPRODUCTION STANDARD FORM 330 (REV. 7/2021) Section D | Page 3

E. Resumes of Key Personnel Proposed for this Contract (Complete one Section E for each key person.) 12 NAME 13 ROLE IN THIS CONTRACT 14 YEARS EXPERIENCE b. WITH CURRENT FIRM Deputy Project Manager: Structures Analysis and Plans a. TOTAL Stephanie Boyd, PE 10 10 15. FIRM NAME AND LOCATION (City and State) Kisinger Campo & Associates, Corp., Tampa, FL 16. EDUCATION (Degree and Specialization) 17. CURRENT AND PROFESSIONAL REGISTRATION (State and Discipline) BSCE, University of Florida Professional Engineer: Florida #88614 18. OTHER PROFESSIONAL QUALIFICATIONS (Publications, Organizations, Training, Awards, etc.) Ms. Boyd has over 10 years of experience in Structural Engineering. She has practical experience in the design and analysis of both concrete and steel bridges, as well as experience in wall design. In addition, she is familiar with the following engineering software: MicroStation, AutoCAD, STAAD, RC-Pier, FB-MultiPier, and PS-Beam. She has been with KCA's Structures department since May 2015. 19. RELEVANT PROJECTS (1) TITLE AND LOCATION (City and State) (2) YEAR COMPLETED Professional Services Construction Maydell Drive over Palm River Bridge Replacement PD&E/Design, Hillsborough County, FL 2021 (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Check if project performed with current firm Structures Engineer. This was a highly successful Hillsborough County/FDOT District Seven LAP project. The project included replacement of the existing 616foot bridge, reconstruction of the approach roadways, drainage improvements, and additional pedestrian connectivity features. The structurally-deficient bridge had been closed to vehicular and pedestrian traffic for several years. The replacement bridge consisted of six 90-foot spans (630-foot total length) with Florida I-36 Beam superstructure founded on prestressed concrete pile bents. Bank and shore rubble riprap was also provided at the sloped abutments for improved scour resistance. The adjacent roadway was reconstructed and a combination open/closed drainage system was provided to accommodate the slightly raised profile. The 10-foot-wide sidewalk on the bridge connects back to the existing sidewalks north and south of the project, however, this wide sidewalk is designed to eventually accommodate the Hillsborough County Trails Master Plan. The KCA Team led all aspects of the PD&E study, final design plans, specification and construction phase support for this project. Extensive permitting efforts were also provided for the USCG Bridge Permit, SWFWMD Permit, USACE Permits (including a 408 Permit), and a Tampa Port Authority Sovereign Submerged Lands Easement. Seeking an accelerated design schedule, KCA successfully guided the County through the Statewide Acceleration Transformation (SWAT) process. During construction, KCA led a proactive coordination approach to assist the contractor to complete construction and open the bridge months ahead of schedule, Fees: \$1.6 m. (2) YEAR COMPLETED (1) TITLE AND LOCATION (City and State) Professional Services Construction Venetian Boulevard Bridge Replacement, City of St. Petersburg, FL 2024 Ongoing (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Structures Engineer. KCA was selected under the City of St. Petersburg's Continuing Services Contract to design and support the Venetian Blvd. Bridge Replacement Project in Shore Acres. The existing bridge was functionally obsolete and deteriorating, requiring structural design, permitting, roadway and drainage reconstruction, seawall repairs, and utility coordination. KCA also designed a pedestrian and vehicular bypass for construction detours and incorporated public feedback to minimize community impact, including using non-displacement piles to reduce vibrations. The bridge design was completed in 2024, with construction starting in February 2025. KCA continues to provide post-design services. (1) TITLE AND LOCATION (City and State) (2) YEAR COMPLETED Professional Services Construction Old Coachman over Alligator Creek - Bridge No. 154252, Pinellas County, FL N/A (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Structures Engineer. Under our Pinellas County Miscellaneous Services contract, KCA was tasked to develop a multi-phased preliminary engineering report (PER) for Old Coachman Road over Alligator Creek to determine the condition of the existing bridge and adjacent structures and propose alternatives for design of a bridge replacement and roadway improvements. The bridge is functionally obsolete and has exceeded its design life. It also exhibits considerable deterioration. The initial bridge replacement study (Phase I) investigated whether these structures within an approximate 10-year period should be replaced, rehabilitated, or repaired. The Phase II replacement study, dated July 2012, addressed preliminary geometry, roadway, and structural design to meet the hydraulic needs of the site, including associated permitting, environmental, and construction considerations. Phase IIB included updated hydraulic modelling of the site and surrounding area based on the most recent City of Clearwater data. KCA proceeded with design of the replacement bridge to raise the profile, remove adjacent aging drainage structures, and improve safety. The hydraulic study and environmental permitting of the complex system was completed to minimize impact on the surrounding natural resources. The final design includes a three-span Florida Slab Beam superstructure for function and rapid construction to minimize road closure during construction.

Structures Engineer. KCA was selected by Pinellas County to provide structural design services for the Madonna Boulevard Bridge Replacement project. KCA is providing structural design services for this bridge replacement that provides sole access to the Monte Cristo community in Tierra Verde. KCA has served the County for the last 36 years, having completed a deck replacement of this very bridge in the early 1990s led by KCA CEO Paul Foley, PE, KCA's Structures Department has designed numerous bridge replacement projects within neighborhoods recently and is aware of the importance of maintaining access for vehicles and pedestrians.

(1) TITLE AND LOCATION (City and State)

Madonna Boulevard Bridge Replacement, Pinellas County, FL

(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE

STANDARD FORM 330 (REV. 7/2021) Continuing Services Contract for Design of FDOT LAP Projects | RFQ: 24-012 Section D | Page 4 AUTHORIZED FOR LOCAL REPRODUCTION

Construction

N/A

Check if project performed with current firm

Professional Services

Ongoing

E. Resumes of Key Personnel Proposed for this Contract (Complete one Section E for each key person.) 12 NAME 13 ROLE IN THIS CONTRACT 14. YEARS EXPERIENCE b. WITH CURRENT FIRM Guillermo Madriz, PE Principal-in-Charge a. TOTAL 28 26 15. FIRM NAME AND LOCATION (City and State) Kisinger Campo & Associates, Corp., Tampa, FL 16. EDUCATION (Degree and Specialization) 17. CURRENT AND PROFESSIONAL REGISTRATION (State and Discipline) MSCE, University of Florida Professional Engineer: Florida, 57530 BSCE, University of Costa Rica 18. OTHER PROFESSIONAL QUALIFICATIONS (Publications, Organizations, Training, Awards, etc.) Mr. Madriz has 28 years of experience in structures design. He currently serves as KCA's Vice President/Director of Corporate Operations and has served as the lead design engineer and project manager for projects that involved highway bridges, foundation analysis and design, retaining walls, and utility structures. Mr. Madriz is experienced in the design of reinforced and prestressed concrete bridges. I-girder and box girder steel bridges, horizontally curved steel bridges, and advanced foundations design. Mr. Madriz has CEI experience including supervision of construction methods and estimation of operating costs. Mr. Madriz assisted with the programming of the FB-Pier software for the University of Florida. 19. RELEVANT PROJECTS (1) TITLE AND LOCATION (City and State) (2) YEAR COMPLETED Professional Services Construction Venetian Boulevard Bridge Replacement, City of St. Petersburg, FL 2024 Ongoing

a. **Structures Oversight.** KCA was selected under the City of St. Petersburg's Continuing Services Contract to design and support the Venetian Blvd. Bridge Replacement Project in Shore Acres. The existing bridge was functionally obsolete and deteriorating, requiring structural design, permitting, roadway and drainage reconstruction, seawall repairs, and utility coordination. KCA also designed a pedestrian and vehicular bypass for construction detours and incorporated public feedback to minimize community impact, including using non-displacement piles to reduce vibrations. The bridge design was completed in 2024, with construction starting in February 2025. KCA continues to provide post-design services.

(1) TITLE AND LOCATION (City and State)

Old Coachman over Alligator Creek - Bridge No. 154252, Pinellas County, FL

(2) YEAR COMPLETED

Professional Services

Construction
N/A

(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE

Check if project performed with current firm

Project Manager. Under our Pinellas County Miscellaneous Services contract, KCA was tasked to develop a multi-phased preliminary engineering report (PER) for Old Coachman Road over Alligator Creek to determine the condition of the existing bridge and adjacent structures and propose alternatives for design of a bridge replacement and roadway improvements. The bridge is functionally obsolete and has exceeded its design life. It also exhibits considerable deterioration. The initial bridge replacement study (Phase I) investigated whether these structures within an approximate 10-year period should be replaced, rehabilitated, or repaired. The Phase II replacement study, dated July 2012, addressed preliminary geometry, roadway, and structural design to meet the hydraulic needs of the site, including associated permitting, environmental, and construction considerations. Phase IIB included updated hydraulic modelling of the site and surrounding area based on the most recent City of Clearwater data. KCA proceeded with design of the replacement bridge to raise the profile, remove adjacent aging drainage structures, and improve safety. The hydraulic study and environmental permitting of the complex system was completed to minimize impact on the surrounding natural resources. The final design includes a three-span Florida Slab Beam superstructure for function and rapid construction to minimize road closure during construction.

(1) TITLE AND LOCATION (City and State)

Fred Howard Park Bridge Replacements, Pinellas County, FL

(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE

(2) YEAR COMPLETED

Professional Services

Construction
N/A

Check if project performed with current firm

Project Manager. KCA prepared final design plans and permitted the replacement of the two deficient structures with new bridges that meet current vehicle and pedestrian design criteria at Fred Howard Park. The bridges, which provide the only access to the causeway on the Gulf of Mexico, were each replaced with a 120-foot long, three-span precast plank bridge with a cast-in-place topping for speed of construction. A combination of underdrains and exfiltration system was used for stormwater treatment. KCA also designed improvements to the adjacent bulkheads and ensured ADA compatibility.

(1) TITLE AND LOCATION (City and State)

Maydell Drive over Palm River Bridge Replacement PD&E/Design, Hillsborough County, FL

(2) YEAR COMPLETED

Professional Services 2021

(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE

(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE

Check if project performed with current firm

Check if project performed with current firm

Structures Design Oversight. This was a highly successful Hillsborough County/FDOT District Seven LAP project. The project included replacement of the existing 616-foot bridge, reconstruction of the approach roadways, drainage improvements, and additional pedestrian connectivity features. The structurally-deficient bridge had been closed to vehicular and pedestrian traffic for several years. The replacement bridge consisted of six 90-foot spans (630-foot total length) with Florida I-36 Beam superstructure founded on prestressed concrete pile bents. Bank and shore rubble riprap was also provided at the sloped abutments for improved scour resistance. The adjacent roadway was reconstructed and a combination open/closed drainage system was provided to accommodate the slightly raised profile. The 10-foot-wide sidewalk on the bridge connects back to the existing sidewalks north and south of the project, however, this wide sidewalk is designed to eventually accommodate the Hillsborough County Trails Master Plan. The KCA Team led all aspects of the PD&E study, final design plans, specification and construction phase support for this project. Extensive permitting efforts were also provided for the USCG Bridge Permit, SWFWMD Permit, USACE Permits (including a 408 Permit), and a Tampa Port Authority Sovereign Submerged Lands Easement. Seeking an accelerated design schedule, KCA successfully guided the County through the Statewide Acceleration Transformation (SWAT) process. During construction, KCA led a proactive coordination approach to assist the contractor to complete construction and open the bridge months ahead of schedule. Fees: \$1.6 million

Continuing Services Contract for Design of FDOT LAP Projects | RFQ: 24-012

AUTHORIZED FOR LOCAL REPRODUCTION

	E.1	Resumes of Key P	ersonnel Proposed for this Co ne Section E for each key person.)	ontract	KISINGER CAMPO
12. NAME		13. ROLE IN THIS CON	• • • • • • • • • • • • • • • • • • • •	14. YEARS EXPERIENCE	& ASSOCIATES
		QA/QC		a. TOTAL	b. WITH CURRENT FIRM
	,			42	36
15. FIRM I	NAME AND LOCATION (City and State)				
Kisinge	r Campo & Associates, Corp., Tampa, FL				
	ATION (Degree and Specialization)	_	17. CURRENT AND PROFESSIONAL REGIST	RATION (State and Discipline)	
	niversity of South Florida		Professional Engineer: Florida #6664		rolina #043693
	R PROFESSIONAL QUALIFICATIONS (Publications		lwards, etc.)		
as QA M manage intercha	ison has 42 years of specialized engineerin anager, Mr. Harrison is responsible for revie ment experience includes leading projects nges, major/minor highway widening, inte I has served as PM and Roadway EOR for so	ewing every submittal to on interstate, state, and rsections, and minor str	o ensure our Quality Control process had local roads. His design experience inc ructures, stormwater, erosion control, to	as been followed and docume ludes all geometric aspects of	ented correctly. His project f interstate highways and
		19.	RELEVANT PROJECTS		
	(1) TITLE AND LOCATION (City and State)			(2) YEAR CO	
	Venetian Boulevard Bridge Replacemen	t, City of St. Petersburg	g, FL	Professional Services 2024	Construction Ongoing
	(3) BRIEF DESCRIPTION (Brief scope, size, cost,	Check if proj	ect performed with current firm		
a.	QA/QC Manager. KCA was selected under Replacement Project in Shore Acres. The eand drainage reconstruction, seawall repaincorporated public feedback to minimize 2024, with construction starting in February	quiring structural design, perr nd vehicular bypass for constr	mitting, roadway ruction detours and		
	(1) TITLE AND LOCATION (City and State)			(2) YEAR CO	
	SR 56 Extension from Meadow Pointe Bo Pasco County, FL	oulevard to US 301 Des	ign-Build, FDOT District Seven	Professional Services 2020	Construction N/A
	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) Al				project performed with current firm
b.	QA/QC. Cone & Graham Inc. (C&G) in ass of the creation of the SR 56 Extension from permitting, utility coordination and reloca ultimate six-lane section and centered in a typical section with frontage roads. The pi	n Meadow Pointe Boule tion, design, drainage, a 250-foot right-of-way	evard to US 301 in Pasco County (incluc and construction). KCA will design the corridor. All ponds will be designed ar	ling providing the right-of-wa four-lane divided typical secti nd constructed to accommoda	ay, PD&E re-evaluation, ion compatible with the ate the ultimate six-lane
	(1) TITLE AND LOCATION (City and State)			(2) YEAR CO	
	Studies and Improvements along the CF	8 611 Corridor, Pinellas	County, FL	Professional Services 2016	Construction N/A
	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) Al	ND SPECIFIC ROLE		Check if	project performed with current firm
C.	Roadway Engineer. Under our Pinellas County Miscellaneous Services contract, KCA has performed stu Pinellas County including a PER for CR 611 at CR 752 Corridor Pedestrian Improvements (KCA studied mi Trail across the intersection of CR 611, CR 752, and the Lake Tarpon outfall canal); a PER for McMullen Bo Bridge Replacement; and Ream Wilson/Pinellas Trail Improvements at Old Coachman Road (KCA will pr ADA-compliant trail and the replacement of a pedestrian bridge that spans the Alligator Creek).			ultiple alignment alternatives oth Road (CR 611) NB Off-Rar	for connecting the Pinellas np over Lake Tarpon Outfall
	(1) TITLE AND LOCATION (City and State)			(2) YEAR CO Professional Services	OMPLETED Construction
	Central Polk Parkway from US 17 (SR 35)	to SR 60 PD&E Study,	FDOT FTE, Polk County, FL	Professional Services	Construction
	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) Al	ND SPECIFIC ROLE		Check if	project performed with current firm
d.	QA/QC. KCA has provided design service SR 35 (US 17). The proposed roadway will six-lane divided, limited-access highway t	be tolled, and the proje	ect includes the design of a gantry for A	All Electronic Toll Collection. K	CA designed an ultimate

six-lane divided, limited-access nighway typical section with a 74-100t median, 12-100t lanes, and 8-100t inside (4-100t paved) and 12-100t outside shoulders (10-foot paved). Although mostly new construction, the bridges over Landfill Road had to be widened to connect existing bridges to the Polk Parkway. The rest of the alignment for the SR 570 to US 17 section includes a tri-level interchange at Polk Parkway, overpasses at SR 540, Thornhill Road (two locations), Old Bartow Eagle Lake Road, and an at-grade intersection at US 17. The KCA team is responsible for roadway design, structures design, drainage design, traffic/S&PM/lighting/signalization, ITS, TTC, utility coordination/SUE, PD&E re-evaluation, environmental permitting, landscape architecture, tolling architecture, tolling structures, public involvement, surveying/R/W mapping, and geotechnical services.

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E. Resumes of Key Personnel Proposed for this Contract (Complete one Section E for each key person.) 12 NAME 13 ROLE IN THIS CONTRACT 14. YEARS EXPERIENCE b. WITH CURRENT FIRM Deborah Hernández, PE Roadway Analysis and Plans EOR a. TOTAL 20 12 15. FIRM NAME AND LOCATION (City and State) Kisinger Campo & Associates, Corp., Tampa, FL 16. EDUCATION (Degree and Specialization) 17. CURRENT AND PROFESSIONAL REGISTRATION (State and Discipline) BM.E., Transportation, University of South Florida (USF) Professional Engineer in Florida #74754 M.S., Engineering Management, USF BSCE. Polytechnic University of Puerto Rico 18. OTHER PROFESSIONAL QUALIFICATIONS (Publications, Organizations, Training, Awards, etc.) Ms. Hernandez is a seasoned Roadway Engineer and Project Manager with 20 years of experience in transportation project management and roadway design. She has successfully led and coordinated projects for FDOT and local government agencies, ensuring innovative, cost-effective, and efficient solutions. Ms. Hernandez's expertise focuses in highway design, safety improvements, and RRR projects. Her technical skills include complex geometric design for interstate highways, roadway widening, intersection improvements, temporary traffic control, signing and pavement marking, cost estimating, and detailed design report preparation. She has also prepared key project documents such as Pavement Design Packages, RRR Reports, Typical Section Packages, and Design Documentation, and is well versed in FDOT design variations, exceptions, and specification package preparation. Ms. Hernandez brings a comprehensive approach to project execution, balancing safety, efficiency, and stakeholder coordination. Since joining KCA in 2013, she has advanced to Deputy Roadway Department Manager, where she continues to drive project success through her leadership and technical expertise. **19. RELEVANT PROJECTS** (1) TITLE AND LOCATION (City and State) (2) YEAR COMPLETED Professional Services SR 56 Extension from Meadow Pointe Boulevard to US 301 Design-Build, FDOT District Seven Construction Pasco County. FL 2020 (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Check if project performed with current firm Senior Roadway Engineer. Cone & Graham Inc. (C&G) in association with KCA was selected by the Florida Department of Transportation District Seven to provide all aspects of the creation of the SR 56 Extension from Meadow Pointe Boulevard to US 301 in Pasco County (including providing the right-of-way, PD&E re-evaluation, permitting, utility coordination and relocation, design, drainage, and construction). KCA will design the four-lane divided typical section compatible with the ultimate six-lane section and centered in a 250-foot right-of-way corridor. All ponds will be designed and constructed to accommodate the ultimate six-lane typical section with frontage roads. The project also includes three new signalized intersections at Meadow Pointe Boulevard, Morris Bridge Road, and US 301. (1) TITLE AND LOCATION (City and State) (2) YEAR COMPLETED

(1) TITLE AND LOCATION (City and State)

Madonna Boulevard Bridge Replacement, Pinellas County, FL

(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE

Senior Roadway Engineer. KCA was selected by Pinellas County to provide structural design services for the Madonna Boulevard Bridge Replacement project

Senior Roadway Engineer. KCA was selected by Pinellas County to provide structural design services for the Madonna Boulevard Bridge Replacement project. KCA is providing structural design services for this bridge replacement that provides sole access to the Monte Cristo community in Tierra Verde. KCA has served the County for the last 36 years, having completed a deck replacement of this very bridge in the early 1990s led by KCA CEO Paul Foley, PE. KCA's Structures Department has designed numerous bridge replacement projects within neighborhoods recently and is aware of the importance of maintaining access for vehicles and pedestrians.

(1) TITLE AND LOCATION (City and State)

Bimini Drive Bridge Replacement, Monroe County, FL

(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE

(2) YEAR COMPLETED

Professional Services
2023

Construction
N/A

Check if project performed with current firm

Roadway Engineer. KCA is providing structures, drainage, and roadway design; environmental assessment and permitting; utility coordination; and public involvement services for the Bimini Drive Bridge (Bridge No. 904603) Replacement project. The Bimini Drive Bridge is one of four historic Duck Key bridges, requiring evaluation under the National Historic Preservation Act (NHPA).

(1) TITLE AND LOCATION (City and State)

Central Polk Parkway from US 17 (SR 35) to SR 60 PD&E Study, FDOT FTE, Polk County, FL

(2) YEAR COMPLETED

Professional Services
2022
Construction
Ongoing

(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE

Check if project performed with current firm

Roadway Engineer. KCA has provided design services for the development of final plans for the new alignment of the Central Polk Parkway from SR 570 (Polk Parkway) to SR 35 (US 17). The proposed roadway will be tolled, and the project includes the design of a gantry for All Electronic Toll Collection. KCA designed an ultimate six-lane divided, limited-access highway typical section with a 74-foot median, 12-foot lanes, and 8-foot inside (4-foot paved) and 12-foot outside shoulders (10-foot paved). Although mostly new construction, the bridges over Landfill Road had to be widened to connect existing bridges to the Polk Parkway. The rest of the alignment for the SR 570 to US 17 section includes a tri-level interchange at Polk Parkway, overpasses at SR 540, Thornhill Road (two locations), Old Bartow Eagle Lake Road, and an at-grade intersection at US 17. The KCA team is responsible for roadway design, structures design, drainage design, traffic/S&PM/lighting/signalization, ITS, TTC, utility coordination/SUE, PD&E re-evaluation, environmental permitting, landscape architecture, tolling architecture, tolling structures, public involvement, surveying/R/W mapping, and geotechnical services.

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	E. R	lesumes of Key P (Complete o	ersonnel Proposed for this Connection Section E for each key person.)	ontract	KISINGER CAMPO	
12. NAME		13. ROLE IN THIS CON	TRACT	14. YEARS EXPERIENCE	u 2000012123	
Dean Grumbach, PE Roadway Ar			and Plans EOR	a. TOTAL	b. WITH CURRENT FIRM	
				9	7	
15. FIRM I	NAME AND LOCATION (City and State)	•		•	•	
Kisinge	r Campo & Associates, Corp., Tampa, FL					
16. EDUC	ATION (Degree and Specialization)		17. CURRENT AND PROFESSIONAL REGIST	RATION (State and Discipline)		
BSCE, U	niversity of Central Florida		Professional Engineer in Florida #919	03		
Mr. Grun	R PROFESSIONAL QUALIFICATIONS (<i>Publications,</i> mbach has over nine years of engineering ex CAD, MicroStation, and GEOPAK.			ision. Mr. Grumbach has experi	ence with software such	
		19.	RELEVANT PROJECTS			
	(1) TITLE AND LOCATION (City and State)			(2) YEAR COM		
	Venetian Boulevard Bridge Replacement	City of St. Petersbur	g, FL	Professional Services 2024	Construction Ongoing	
	(3) BRIEF DESCRIPTION (Brief scope, size, cost, e	tc.) AND SPECIFIC ROLE		· · · · · · · · · · · · · · · · · · ·	t performed with current firm	
Roadway Engineer. KCA was selected under the City of St. Petersburg's Continuing Services Contract to design and support the Venetian Blvd. Bric Replacement Project in Shore Acres. The existing bridge was functionally obsolete and deteriorating, requiring structural design, permitting, roadw and drainage reconstruction, seawall repairs, and utility coordination. KCA also designed a pedestrian and vehicular bypass for construction detours incorporated public feedback to minimize community impact, including using non-displacement piles to reduce vibrations. The bridge design was a 2024, with construction starting in February 2025. KCA continues to provide post-design services.					itting, roadway ction detours and	
	(1) TITLE AND LOCATION (City and State)	(2) YEAR CON Professional Services	MPLETED Construction			
	Madonna Boulevard Bridge Replacement	Ongoing	N/A			
	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AN	Check if p	roject performed with current firm			
b.	Roadway Engineer. KCA was selected by is providing structural design services for the County for the last 36 years, having cor Department has designed numerous bridg vehicles and pedestrians.	his bridge replacemen npleted a deck replace	t that provides sole access to the Monte ement of this very bridge in the early 19	e Cristo community in Tierra Ve 190s led by KCA CEO Paul Foley	rde. KCA has served , PE. KCA's Structures	
	(1) TITLE AND LOCATION (City and State)			(2) YEAR COM		
	Central Polk Parkway from US 17 (SR 35)	Professional Services 2022	Construction Ongoing			
	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AN	Check if p	roject performed with current firm			
C.	Roadway Engineer. KCA has provided design services for the development of final plans for the new alignment of the Central Polk Parkway from SR 570 (Polk Parkway) to SR 35 (US 17). The proposed roadway will be tolled, and the project includes the design of a gantry for All Electronic Toll Collection. KCA designed an ultimate six-lane divided, limited-access highway typical section with a 74-foot median, 12-foot lanes, and 8-foot inside (4-foot paved) and 12-foot outside shoulders (10-foot paved). Although mostly new construction, the bridges over Landfill Road had to be widened to connect existing bridges to the Polk Parkway. The rest of the alignment for the SR 570 to US 17 section includes a tri-level interchange at Polk Parkway, overpasses at SR 540, Thornhill Road (two locations), Old Bartow Eagle Lake Road, and an at-grade intersection at US 17. The KCA team is responsible for roadway design, structures design, drainage design, traffic/S&PM/lighting/signalization, ITS, TTC, utility coordination/SUE, PD&E re-evaluation, environmental permitting, landscape architecture, tolling architecture, tolling structures, public involvement, surveying/R/W mapping, and geotechnical services.					
	(1) TITLE AND LOCATION (City and State)			(2) YEAR CON		
	US 92/SR 600/Gandy Boulevard (East of FDOT District Seven, Hillsborough and Pi		ore Boulevard) PD&E and Design,	Professional Services Ongoing	Construction N/A	
	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AN	•			roject performed with current firm	
d.	Roadway Engineer. KCA was selected by FDOT District Seven to conduct a PD&E study for Gandy Boulevard from East of 4th Street North in Pinellas County to Westshore Boulevard in Hillsborough County as well as prepare 15% Line and Grade design for the project segment from 4th Street North to west of the Gandy Bridge. The contract has an option for additional design services necessary to accelerate the project development. This project's purpose is to reduce traffic congestion and improve bicycle and pedestrian accommodations along Gandy Boulevard including the existing eastbound and westbound bridges. The improvements will extend the existing controlled access facility on Gandy Boulevard and connect to the Selmon West Extension in Hillsborough County. The 7-mile, four-lane divided facility is classified as an urban principal arterial and is part of FDOT's Strategic Intermodal System (SIS). The PD&E will evaluate improvements including grade separations at major intersections and widening Gandy Boulevard to six lanes. Bridge widening and/or replacement will also be evaluated as part of this project. KCA will develop three alternatives, analyze the need for managed lanes, and assess the project's impact on the social, economic, cultural, natural, and physical environment.					

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	E. Re	sumes of Key Po (Complete or	ersonnel Proposed for this Cone Section E for each key person.)	ontract	KISINGER CAMPO
12. NAME		13. ROLE IN THIS CON	TRACT	14. YEARS EXPERIENCE	
Austin Black, PE		Structures Analysis	and Plans	a. TOTAL	b. WITH CURRENT FIRM
				11	10.5
15. FIRM N	IAME AND LOCATION (City and State)				
Kisinger	Campo & Associates, Corp., Tampa, FL				
16. EDUCA	ATION (Degree and Specialization)		17. CURRENT AND PROFESSIONAL REGIST	RATION (State and Discipline)	
	orida State University orida State University		Professional Engineer: Florida #8348:	7	
18. OTHER	R PROFESSIONAL QUALIFICATIONS (Publications, O	rganizations, Training, A	wards, etc.)		
as well a	k has 11 years of engineering experience. His e s analysis of LFR and LRFR for a variety of tyl TCulvert, Conspan, and MDX.				
		19.	RELEVANT PROJECTS		
	(1) TITLE AND LOCATION (City and State)			(2) YEAR CO	
	62nd Avenue Bridge Rehabilitation, City of	St. Petersburg, FL		Professional Services Ongoing	Construction
	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE			ct performed with current firm
u.	Project Manager. This project consists of the access to the Bayway Isles community in St. deteriorated at an accelerated rate compared this bridge to be a part of its Local Agency P replacement over navigable waters and in ar Fees: \$494,226	Petersburg, Florida. I d to conventional pre rogram. KCA has peri	The 40-year-old bridge was originally on stressed beam bridges. As a result, the formed all structures, roadway, and MO	onstructed with hollow core s City of St. Petersburg along w T design for this project. Due	lab units, which have vith FDOT has selected to the superstructure
	(1) TITLE AND LOCATION (City and State)			(2) YEAR CO Professional Services	MPLETED Construction
	Madonna Boulevard Bridge Replacement, I	Ongoing	N/A		
Σ.	(3) BRIEF DESCRIPTION (<i>Brief scope, size, cost, etc.</i>) AND Drainage Engineer. KCA designed a 2.6-mile linking North Hyde Park from Howard and Al track begins at Howard Avenue and Cass Str. Downtown Phase I East Cass Street and East Phase I project, the cycle track follows Nuccional City of Tampa's InVision Center City Plan. Fe	e cycle track to provice rmenia Avenues as w eet near the Armory/ Tyler Street Two-Wa o Parkway into Ybor	rell as Tampa Heights, Cuscaden Park, a JCC. It then follows Cass Street into Do y Conversion project. At Nebraska Avei	mpa along Cass Street, Nuccio nd Ybor City to the Hillsborou wntown meeting the existing nue, the existing eastern termi	gh River. The cycle western terminus of the inus of the Downtown
	(1) TITLE AND LOCATION (City and State)			(2) YEAR CO Professional Services	MPLETED Construction
	Venetian Boulevard Bridge Replacement, C	City of St. Petersburg	g, FL	2024	Ongoing
	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND	SPECIFIC ROLE		Check if p	project performed with current firm
Structures Engineer. KCA was selected under the City of St. Petersburg's Continuing Services Contract to design and support the Venetian Blvd. Bridg Replacement Project in Shore Acres. The existing bridge was functionally obsolete and deteriorating, requiring structural design, permitting, roadway and drainage reconstruction, seawall repairs, and utility coordination. KCA also designed a pedestrian and vehicular bypass for construction detours an incorporated public feedback to minimize community impact, including using non-displacement piles to reduce vibrations. The bridge design was com 2024, with construction starting in February 2025. KCA continues to provide post-design services.				nitting, roadway uction detours and design was completed in	
	(1) TITLE AND LOCATION (City and State)		nn 1894 ann an Canada El	(2) YEAR CO Professional Services	MPLETED Construction
	Maydell Drive over Palm River Bridge Repla		gn, Hillsborough County, FL	2021	N/A
	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND Structures Engineer This was a highly succession.		OUNTY/FDOT District Seven LAP project	· ·	project performed with current firm
Structures Engineer. This was a highly successful Hillsborough County/FDOT District Seven LAP project. The project included replacement of foot bridge, reconstruction of the approach roadways, drainage improvements, and additional pedestrian connectivity features. The structura had been closed to vehicular and pedestrian traffic for several years. The replacement bridge consisted of six 90-foot spans (630-foot total left) labeled to the superstructure founded on prestressed concrete pile bents. Bank and shore rubble riprap was also provided at the sloped abutme scour resistance. The adjacent roadway was reconstructed and a combination open/closed drainage system was provided to accommodate the profile. The 10-foot-wide sidewalk on the bridge connects back to the existing sidewalks north and south of the project, however, this wide sit to eventually accommodate the Hillsborough County Trails Master Plan. The KCA Team led all aspects of the PD&E study, final design plans, seen support for this project. Extensive permitting efforts were also provided for the USCG Bridge Permit, SWFWMD Permit, U (including a 408 Permit), and a Tampa Port Authority Sovereign Submerged Lands Easement. Seeking an accelerated design schedule, KCA the County through the Statewide Acceleration Transformation (SWAT) process. During construction, KCA led a proactive coordination appro				cructurally-deficient bridge total length) with Florida butments for improved odate the slightly raised wide sidewalk is designed blans, specification and rmit, USACE Permits e, KCA successfully guided	

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E. Resumes of Key Personnel Proposed for this Contract (Complete one Section E for each key person.) 12 NAME 13 ROLE IN THIS CONTRACT 14. YEARS EXPERIENCE a. TOTAL b. WITH CURRENT FIRM Dr. Fathy Abdalla. PE. PTOE S&PM/Lighting Analysis and Plans EOR 29 21 15. FIRM NAME AND LOCATION (City and State) Kisinger Campo & Associates, Corp., Tampa, FL 16. EDUCATION (Degree and Specialization) 17. CURRENT AND PROFESSIONAL REGISTRATION (State and Discipline) Professional Engineer: Florida #63914. North Carolina #043459. Michigan #6201051908: Ph.D., Civil Engineering (Transportation), University of Central Florida MSCE, Cairo University Texas #148760 BSCE, Zagazig University Professional Traffic Operations Engineer #1578

18. OTHER PROFESSIONAL QUALIFICATIONS (Publications, Organizations, Training, Awards, etc.)

Dr. Abdalla has 29 years of diversified experience in the transportation industry and serves as KCA's Traffic Engineering and Planning Department Manager. Traffic experience includes traffic impact studies, traffic circulation studies, traffic simulation and modeling, intersection/interchange analysis, corridor analysis, traffic safety, signing and pavement marking design and plan preparation, and signalization design. He has prepared numerous transportation/traffic studies for FDOT and other local authorities. Typical traffic studies include trip generation, trip distribution, level of service/capacity analyses of signalized intersections and arterials, freeway merge, diverge, and weaving analyses along with the associated recommendations for the improvements necessary to meet design criteria. Roadway experience includes roadway plan and profile design, typical section design, pavement design, and preparing exceptions/Variations. Dr. Abdalla has served as Project Engineer for different City/County/FDOT roadway design projects in Florida, including new construction, widening, and resurfacing projects.

19. RELEVANT PROJECTS (1) TITLE AND LOCATION (City and State) (2) YEAR COMPLETED Professional Services Construction Maydell Drive over Palm River Bridge Replacement PD&E/Design, Hillsborough County, FL 2021 Check if project performed with current firm (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Senior Traffic Engineer. This was a highly successful Hillsborough County/FDOT District Seven LAP project. The project included replacement of the existing 616-foot bridge, reconstruction of the approach roadways, drainage improvements, and additional pedestrian connectivity features. The structurally-deficient bridge had been closed to vehicular and pedestrian traffic for several years. The replacement bridge consisted of six 90-foot spans (630-foot total length) with Florida I-36 Beam superstructure founded on prestressed concrete pile bents. Bank and shore rubble riprap was also provided at the sloped abutments for improved scour resistance. The adjacent roadway was reconstructed and a combination open/closed drainage system was provided to accommodate the slightly raised profile. The 10-foot-wide sidewalk on the bridge connects back to the existing sidewalks north and south of the project, however, this wide sidewalk is designed to eventually accommodate the Hillsborough County Trails Master Plan. The KCA Team led all aspects of the PD&E study, final design plans, specification and construction phase support for this project. Extensive permitting efforts were also provided for the USCG Bridge Permit, SWFWMD Permit, USACE Permits (including a 408 Permit), and a Tampa Port Authority Sovereign Submerged Lands Easement. Seeking an accelerated design schedule, KCA successfully guided the County through the Statewide Acceleration Transformation (SWAT) process. During construction, KCA led a proactive coordination approach to assist the contractor to complete construction and open the bridge months ahead of schedule. Fees: \$1.6 million (1) TITLE AND LOCATION (City and State) (2) YEAR COMPLETED SR 56 Extension from Meadow Pointe Boulevard to US 301 Design-Build. Professional Services Construction FDOT District Seven, Pasco County, FL 2020 N/A (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Check if project performed with current firm Lead Traffic Engineer. Cone & Graham Inc. (C&G) in association with KCA was selected by the Florida Department of Transportation District Seven to provide all aspects of the creation of the SR 56 Extension from Meadow Pointe Boulevard to US 301 in Pasco County (including providing the right-of-way, PD&E reevaluation, permitting, utility coordination and relocation, design, drainage, and construction). KCA will design the four-lane divided typical section compatible with the ultimate six-lane section and centered in a 250-foot right-of-way corridor. All ponds will be designed and constructed to accommodate the ultimate six-lane typical section with frontage roads. The project also includes three new signalized intersections at Meadow Pointe Boulevard, Morris Bridge Road, and US 301. (1) TITLE AND LOCATION (City and State) (2) YEAR COMPLETED Professional Services Construction Fred Howard Park Bridge Replacements, Pinellas County, FL 2012 N/A (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Check if project performed with current firm Traffic Engineer. KCA prepared final design plans and permitted the replacement of the two deficient structures with new bridges that meet current vehicle and pedestrian design criteria at Fred Howard Park. The bridges, which provide the only access to the causeway on the Gulf of Mexico, were each replaced with a 120-foot long, three-span precast plank bridge with a cast-in-place topping for speed of construction. A combination of underdrains and exfiltration system was used for stormwater treatment. KCA also designed improvements to the adjacent bulkheads and ensured ADA compatibility. (2) YEAR COMPLETED Professional Services Construction

(3) BRIEF DESCRIPTION (*Brief scope, size, cost, etc.*) AND SPECIFIC ROLE **Traffic Engineer.** KCA has provided design services for the development of final plans for the new alignment of the Central Polk Parkway from SR 570 (Polk Parkway) to SR 35 (US 17). The proposed roadway will be tolled, and the project includes the design of a gantry for All Electronic Toll Collection. KCA designed an ultimate six-lane divided, limited-access highway typical section with a 74-foot median, 12-foot lanes, and 8-foot inside (4-foot paved) and 12-foot outside shoulders (10-foot paved). Although mostly new construction, the bridges over Landfill Road had to be widened to connect existing bridges to the Polk Parkway. The rest of the alignment for the SR 570 to US 17 section includes a tri-level interchange at Polk Parkway, overpasses at SR 540, Thornhill Road (two locations), Old Bartow Eagle Lake Road, and an at-grade intersection at US 17. The KCA team is responsible for roadway design, structures design, drainage design, traffic/S&PM/lighting/signalization, ITS, TTC, utility coordination/SUE, PD&E re-evaluation, environmental permitting, landscape architecture, tolling architecture, tolling structures, public involvement, surveying/R/W mapping, and geotechnical services.

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Central Polk Parkway from US 17 (SR 35) to SR 60 PD&E Study. FDOT FTE. Polk County. FL

Ongoing

2022

E. Resumes of Key Personnel Proposed for this Contract (Complete one Section E for each key person.) 12 NAME 13 ROLE IN THIS CONTRACT 14. YEARS EXPERIENCE b. WITH CURRENT FIRM Scott Samuels, PE S&PM/Lighting Analysis and Plans EOR a. TOTAL 2 10 15. FIRM NAME AND LOCATION (City and State) Kisinger Campo & Associates, Corp., Tampa, FL 17. CURRENT AND PROFESSIONAL REGISTRATION (State and Discipline) 16. EDUCATION (Degree and Specialization) BSCE. University of South Florida Professional Engineer: Florida #88738 18. OTHER PROFESSIONAL QUALIFICATIONS (Publications, Organizations, Training, Awards, etc.) Mr. Samuels has over 10 years of diversified experience in traffic engineering including signing and pavement markings, signalization, lighting, ITS, and analysis and design. He has designed and served as Engineer-of-Record (EOR) on several projects as a Traffic Engineer for the Florida Department of Transportation (FDOT) and local Florida municipalities. He has experience with several software programs including OED. MicroStation, FDOT Connect. AGi32, and ArcGIS. 19. RELEVANT PROJECTS (1) TITLE AND LOCATION (City and State) (2) YEAR COMPLETED Professional Services Construction Central Polk Parkway from US 17 (SR 35) to SR 60 PD&E Study, FDOT FTE, Polk County, FL 2022 Ongoing (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Check if project performed with current firm Traffic Analysis, Signalization, and S&PM. KCA has provided design services for the development of final plans for the new alignment of the Central Polk Parkway from SR 570 (Polk Parkway) to SR 35 (US 17). The proposed roadway will be tolled, and the project includes the design of a gantry for All Electronic Toll Collection. KCA designed an ultimate six-lane divided, limited-access highway typical section with a 74-foot median, 12-foot lanes, and 8-foot inside (4-foot payed) and 12-foot outside shoulders (10-foot payed). Although mostly new construction, the bridges over Landfill Road had to be widened to connect existing bridges to the Polk Parkway. The rest of the alignment for the SR 570 to US 17 section includes a tri-level interchange at Polk Parkway, overpasses at SR 540, Thornhill Road (two locations), Old Bartow Eagle Lake Road, and an at-grade intersection at US 17. The KCA team is responsible for roadway design, structures design, drainage design, traffic/S&PM/lighting/signalization, ITS, TTC, utility coordination/SUE, PD&E re-evaluation, environmental permitting, landscape architecture, tolling architecture, tolling structures, public involvement, surveying/R/W mapping, and geotechnical services. TBNext Segment 2: I-275 (SR 93) from north of I-375/5th Avenue N to north of 38th Ave. North. Professional Services Construction FDOT District Seven, Pinellas County, FL Ongoing N/A (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Check if project performed with current firm Traffic Engineer, Segment 2 of TBNext (I-275 from North of I-375/5th Avenue N. to North of 38th Avenue North) is an Urban Principal Arterial Interstate with 3 interchanges and 11 bridges. This project's improvements include the addition of express lanes in the median of I-275, operational and lane continuity improvements, rigid pavement rehabilitation, Regional Rapid Transit (RRT) accommodations, and noise wall construction. In addition, KCA is evaluating and addressing safety issues (i.e., hydroplaning and wrong way detection [WWD]), providing a comprehensive temporary traffic control plan (TTCP), permitting, and maintaining current bridge clearances are all essential requirements. Fees: \$9.6 million (1) TITLE AND LOCATION (City and State) (2) YEAR COMPLETED Professional Services Construction Charlotte County ATMS/ITS Master Plan, FDOT District One, Charlotte County, FL Ongoing N/A (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Check if project performed with current firm Traffic/ITS Project Engineer; Signal and ATMS Engineer. This project involves development of an Advanced Traffic Management Systems (ATMS)/Intelligent Transportation Systems (ITS) Master Plan for Charlotte County including the Charlotte County Traffic Management Center (TMC), all signalized intersections (existing and proposed), all systems and networks tied to and relating to the signal systems, all ITS device locations (existing and proposed), and the associated communication routes (existing and proposed) within Charlotte County. Services also include ATMS/ITS study, planning, design, integration, management, review, and evaluation of the Incident Management (TIM) plans. The ATMS/ITS Master Plan will be consistent with the current technology regarding Connected and Automated Vehicle (CAV) programs. Fees: \$479,288 (1) TITLE AND LOCATION (City and State) (2) YEAR COMPLETED Professional Services Construction Cape Coral Bridge Design/Owner's Representative, Lee County, FL Ongoing N/A (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Check if project performed with current firm Traffic/ITS Project Engineer; The KCA team was selected to perform Design and Owner's Representative services for the westbound span replacement and eastbound span widening of the Cape Coral Bridge between Cape Coral and Fort Myers in Lee County. The key aspects of this project include development of a design that expands the existing facility to six lanes while minimizing right-of-way (R/W) impacts, maintains westbound access for residents on the Cape Coral causeway, incorporates sheltered sidewalks on each bridge that tie into pedestrian facilities on Cape Coral Parkway and College Parkway, and maintains four

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lanes of traffic and local resident access throughout construction. The final design will focus on safety for both vehicular and pedestrian traffic. The design will also have the vision to incorporate high-value aesthetic components into the entire project to provide the "Signature Gateway" to the City of Cape Coral that is

consistent with Lee County and the City of Cape Coral's goals for the project. Cost: \$3.5 million

	E. R	esumes of Key Po (Complete or	ersonnel Proposed for this Cone Section E for each key person.)	ontract	KISINGER CAMPO 8 ASSOCIATES	
12. NAME		13. ROLE IN THIS CON	TRACT	14. YEARS EXPERIENCE		
Nicole Selly, ENV SP		Public Involvement	Task Manager	a. TOTAL	b. WITH CURRENT FIRM	
				19	6	
15. FIRM N	NAME AND LOCATION (City and State)	·		•		
Kisinge	r Campo & Associates, Corp., Tampa, FL					
16. EDUC	ATION (Degree and Specialization)		17. CURRENT AND PROFESSIONAL REGIST	RATION (State and Discipline)		
BS, Biolo	ogy University of Texas		Envision Sustainability Professional: ‡	#51773		
18. OTHER	R PROFESSIONAL QUALIFICATIONS (Publications,	Organizations, Training, A	wards, etc.)			
(FDOT) I Decision many NI Finding	r has 19 years of experience in National Envi District Seven PD&E Section. During her time Making (ETDM) Coordinator, Cultural Resou EPA documents, including, Type 1 Categorica on No Significant Impacts. Project managen experience in biological research and monit	e at FDOT, she had serv urces Coordinator and o al Exclusions, Type 2 Ca nent included coordina	ved as the District Specialist on listed sp Contamination Specialist. She has beer ategorical Exclusions, State Environmen ation with county and local governmen	pecies, wetlands, and habitat, I n involved in development, coo ntal Impact Reports, and Envir ts and PD&E document review.	Efficient Transportation ordination, and review of conmental Assessment/	
		19.	RELEVANT PROJECTS			
	(1) TITLE AND LOCATION (City and State)			(2) YEAR CON		
	62nd Avenue Bridge Rehabilitation, City	Professional Services Ongoing	Construction			
	(3) BRIEF DESCRIPTION (Brief scope, size, cost, e.	Check if project	ct performed with current firm			
a.	Senior Environmental Specialist. This pro 157408 is the only access to the Bayway Islaunits, which have deteriorated at an accele FDOT has selected this bridge to be a part of superstructure replacement over navigable in-house. Fees: \$494,226	ge was originally constructed tes. As a result, the City of St. Pos, roadway, and MOT design fo	with hollow core slab etersburg along with r this project. Due to the			
	(1) TITLE AND LOCATION (City and State)	(2) YEAR CON				
	Venetian Boulevard Bridge Replacement,	Professional Services 2024	Construction Ongoing			
	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) ANI	Check if p	project performed with current firm			
b.	Public Involvement. KCA was selected under the City of St. Petersburg's Continuing Services Contract to design and support the Venetian Blvd. Bridge Replacement Project in Shore Acres. The existing bridge was functionally obsolete and deteriorating, requiring structural design, permitting, roadway and drainage reconstruction, seawall repairs, and utility coordination. KCA also designed a pedestrian and vehicular bypass for construction detours and incorporated public feedback to minimize community impact, including using non-displacement piles to reduce vibrations. The bridge design was comple 2024, with construction starting in February 2025. KCA continues to provide post-design services.					
	(1) TITLE AND LOCATION (City and State)			(2) YEAR CON		
	Madonna Boulevard Bridge Replacement	, Pinellas County, FL		Professional Services Ongoing	Construction N/A	
	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AN	D SPECIFIC ROLE		'	roject performed with current firm	
C.	Senior Environmental Scientist/Public Involvement. KCA was selected by Pinellas County to provide structural design services for the Madonna Boulevard Bridge Replacement project. KCA is providing structural design services for this bridge replacement that provides sole access to the Monte Cristo community in Tierra Verde. KCA has served the County for the last 36 years, having completed a deck replacement of this very bridge in the early 1990s led by KCA CEO Paul Foley, PE. KCA's Structures Department has designed numerous bridge replacement projects within neighborhoods recently and is aware of the importance of maintaining access for vehicles and pedestrians.					
	(1) TITLE AND LOCATION (City and State)			(2) YEAR CON Professional Services	MPLETED Construction	
	Maydell Drive over Palm River Bridge Rep	lacement PD&E/Desi	gn, Hillsborough County, FL	2021	N/A	
	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AN	D SPECIFIC ROLE		Check if p	roject performed with current firm	
d.	Senior Environmental Scientist. This was a highly successful Hillsborough County/FDOT District Seven LAP project. The project included replacement of the existing 616-foot bridge, reconstruction of the approach roadways, drainage improvements, and additional pedestrian connectivity features. The structurally-deficient bridge had been closed to vehicular and pedestrian traffic for several years. The replacement bridge consisted of six 90-foot spans (630-foot total length) with Florida I-36 Beam superstructure founded on prestressed concrete pile bents. Bank and shore rubble riprap was also provided at the sloped abutments for improved scour resistance. The adjacent roadway was reconstructed and a combination open/closed drainage system was provided to accommodate the slightly raised profile. The 10-foot-wide sidewalk on the bridge connects back to the existing sidewalks north and south of the project, however, this wide sidewalk is designed to eventually accommodate the Hillsborough County Trails Master Plan. The KCA Team led all aspects of the PD&E study final design plans, specification and construction phase support for this project. Extensive permitting efforts were also provided for the USCG Bridge Permit, SWFWMD Permit, USACE Permits (including a 408 Permit), and a Tampa Port Authority Sovereign Submerged Lands Easement. Seeking an accelerated design schedule, KCA successfully guided the County through the Statewide Acceleration Transformation (SWAT) process. During construction, KCA led a proactive					
	schedule, KCA successfully guided the Coul coordination approach to assist the contrac	ity through the Statew ttor to complete constr	viue Acceleration Transformation (SWA ruction and open the bridge months ah	a) process. During construction ead of schedule. Fees : \$1.6 mil	i, KCA ied a proactive lion	

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E. Resumes of Key Personnel Proposed for this Contract (Complete one Section E for each key person.) 12 NAME 13. ROLE IN THIS CONTRACT 14. YEARS EXPERIENCE b. WITH CURRENT FIRM **Diana Albarracin** Public Involvement a. TOTAL 5 21 15. FIRM NAME AND LOCATION (City and State) Kisinger Campo & Associates, Corp., Tampa, FL 16. EDUCATION (Degree and Specialization) 17. CURRENT AND PROFESSIONAL REGISTRATION (State and Discipline) MCE, University of South Florida; BSCE, Columbia School of Engineering 18. OTHER PROFESSIONAL QUALIFICATIONS (Publications, Organizations, Training, Awards, etc.) Ms. Albarracin has 21 years of diverse experience in Civil Engineering and Transportation Engineering, including development of traffic design for use with design and PD&E studies, traffic simulation, development of planning and traffic studies, signalization plans, communication plans, intelligent transportation systems (ITS) device plans, preparation of Engineer's Construction Cost Estimate, Access Management studies, and lighting justification studies. She also has knowledge of VISSIM, HCS, CORSIM, Microstation V8i, Synchro, OTISS - Traffic Impact Study software, Crash Data Management System (CDMS), Arc GIS and Windows Vista... 19. RELEVANT PROJECTS (1) TITLE AND LOCATION (City and State) (2) YEAR COMPLETED Professional Services Construction Old Coachman over Alligator Creek - Bridge No. 154252, Pinellas County, FL 2022 Ongoing (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Check if project performed with current firm **PD&E Designer.** Under our Pinellas County Miscellaneous Services contract, KCA was tasked to develop a multi-phased preliminary engineering report (PER) for Old Coachman Road over Alligator Creek to determine the condition of the existing bridge and adjacent structures and propose alternatives for design of a bridge replacement and roadway improvements. The bridge is functionally obsolete and has exceeded its design life. It also exhibits considerable deterioration. The initial bridge replacement study (Phase I) investigated whether these structures within an approximate 10-year period should be replaced, rehabilitated, or repaired. The Phase II replacement study, dated July 2012, addressed preliminary geometry, roadway, and structural design to meet the hydraulic needs of the site, including associated permitting, environmental, and construction considerations. Phase IIB included updated hydraulic modelling of the site and surrounding area based on the most recent City of Clearwater data. KCA proceeded with design of the replacement bridge to raise the profile, remove adjacent aging drainage structures, and improve safety. The hydraulic study and environmental permitting of the complex system was completed to minimize impact on the surrounding natural resources. The final design includes a three-span Florida Slab Beam superstructure for function and rapid construction to minimize road closure during construction. (1) TITLE AND LOCATION (City and State) (2) YEAR COMPLETED Professional Services Construction Venetian Boulevard Bridge Replacement, City of St. Petersburg, FL 2024 Ongoing (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Check if project performed with current firm Public Involvement. KCA was selected under the City of St. Petersburg's Continuing Services Contract to design and support the Venetian Blvd. Bridge Replacement Project in Shore Acres. The existing bridge was functionally obsolete and deteriorating, requiring structural design, permitting, roadway and drainage reconstruction, seawall repairs, and utility coordination, KCA also designed a pedestrian and vehicular bypass for construction detours and incorporated public feedback to minimize community impact, including using non-displacement piles to reduce vibrations. The bridge design was completed in 2024, with construction starting in February 2025. KCA continues to provide post-design services (2) YEAR COMPLETED

(1) TITLE AND LOCATION (City and State) Professional Services

Central Polk Parkway from US 17 (SR 35) to SR 60 PD&E Study, FDOT FTE, Polk County, FL

2022 N/A Check if project performed with current firm

(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE

PD&E Designer. KCA has provided design services for the development of final plans for the new alignment of the Central Polk Parkway from SR 570 (Polk Parkway) to SR 35 (US 17). The proposed roadway will be tolled, and the project includes the design of a gantry for All Electronic Toll Collection. KCA designed an ultimate six-lane divided, limited-access highway typical section with a 74-foot median, 12-foot lanes, and 8-foot inside (4-foot paved) and 12-foot outside shoulders (10-foot paved). Although mostly new construction, the bridges over Landfill Road had to be widened to connect existing bridges to the Polk Parkway. The rest of the alignment for the SR 570 to US 17 section includes a tri-level interchange at Polk Parkway, overpasses at SR 540, Thornhill Road (two locations), Old Bartow Eagle Lake Road, and an at-grade intersection at US 17. The KCA team is responsible for roadway design, structures design, drainage design, traffic/S&PM/lighting/signalization, ITS, TTC, utility coordination/SUE, PD&E re-evaluation, environmental permitting, landscape architecture, tolling architecture, tolling structures, public involvement, surveying/R/W mapping, and geotechnical services.

(2) YEAR COMPLETED (1) TITLE AND LOCATION (City and State) Professional Services Construction Tubby's Creek and Mosquito Creek Bridge Replacements, Monroe County, FL Ongoing N/A

(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE

Check if project performed with current firm

Construction

PD&E Designer. This project consists of the replacement of the existing Tubby's Creek Bridge and Mosquito Creek Bridge along Card Sound Road. The existing bridges each consist of a three-span hollow core slab unit bridge founded on pile bents that serves as a hurricane evacuation rout for residents that live in the Ocean Reef community as well as all the southern Keys. The existing superstructures and substructures were in poor condition as a result of the structure's age and the extremely aggressive environment present at the bridge site. Monroe County tasked KCA to design bridge replacements that will provide a full 75 year service life. The bridges will utilize three 44ft spans for a total bridge length of 132 feet and services two 12-foot lanes with 8-foot outside shoulders and single slope traffic barriers. Because these bridges provide essential access to other areas in the Keys, a phased construction concept was required which will allow a single lane of traffic to be open during the entirety of construction. Special preference was given to corrosion resistant materials and special concrete mix designs to ensure this bridge replacement achieves the full 75-year design life. In addition to structures and roadway design, KCA provided MOT, S&PM, and drainage design as well as environmental/permitting services and public involvement for the project. Survey, geotechnical engineering, and coastal engineering have been provided by our subconsultants.

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E. Resumes of Key Personnel Proposed for this Contract (Complete one Section E for each key person.) 12 NAME 13 ROLE IN THIS CONTRACT 14. YEARS EXPERIENCE b. WITH CURRENT FIRM Tara Spieler, PE Drainage/Hydraulics Analysis and Plans EOR a. TOTAL 29 23 15. FIRM NAME AND LOCATION (City and State) Kisinger Campo & Associates, Corp., Tampa, FL 16. EDUCATION (Degree and Specialization) 17. CURRENT AND PROFESSIONAL REGISTRATION (State and Discipline) MS, Geology, University of South Florida Professional Engineer in Florida #55333; Georgia #32515; North Carolina # 043494 BSCE, University of South Florida BS, Geology, University of South Florida 18. OTHER PROFESSIONAL QUALIFICATIONS (Publications, Organizations, Training, Awards, etc.) Ms. Spieler has 29 years of extensive experience in stormwater design and permitting. She has an extensive background in geology and civil engineering, with considerable knowledge of both surface and groundwater flow systems. She has performed stormwater design and permitting for various municipalities, private clients and FDOT. Ms. Spieler has completed pond siting reports, bridge hydraulic reports, scour evaluations and designed stormsewer systems and pond sites. Ms. Spieler is familiar with many computer programs for hydrology and hydraulic modeling, including HY-8, AdICPR, HEC-RAS, ASAD, and PONDS. FDEP Stormwater Inspector. 19. RELEVANT PROJECTS (1) TITLE AND LOCATION (City and State) (2) YEAR COMPLETED Professional Services Construction Venetian Boulevard Bridge Replacement, City of St. Petersburg, FL 2024 Ongoing (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Check if project performed with current firm Senior Drainage Engineer. KCA was selected under the City of St. Petersburg's Continuing Services Contract to design and support the Venetian Blvd. Bridge Replacement Project in Shore Acres. The existing bridge was functionally obsolete and deteriorating, requiring structural design, permitting, roadway and drainage reconstruction, seawall repairs, and utility coordination. KCA also designed a pedestrian and vehicular bypass for construction detours and incorporated public feedback to minimize community impact, including using non-displacement piles to reduce vibrations. The bridge design was completed in 2024, with construction starting in February 2025. KCA continues to provide post-design services. SR 56 Extension from Meadow Pointe Boulevard to US 301 Design-Build, FDOT District Seven Professional Services Construction Pasco County, FL 2020 N/A (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Check if project performed with current firm Stormwater Engineer. Cone & Graham Inc. (C&G) in association with KCA was selected by the Florida Department of Transportation District Seven to provide all aspects of the creation of the SR 56 Extension from Meadow Pointe Boulevard to US 301 in Pasco County (including providing the right-of-way, PD&E re-

Stormwater Engineer. Cone & Graham Inc. (C&G) in association with KCA was selected by the Florida Department of Transportation District Seven to provide all aspects of the creation of the SR 56 Extension from Meadow Pointe Boulevard to US 301 in Pasco County (including providing the right-of-way, PD&E re-evaluation, permitting, utility coordination and relocation, design, drainage, and construction). KCA will design the four-lane divided typical section compatible with the ultimate six-lane section and centered in a 250-foot right-of-way corridor. All ponds will be designed and constructed to accommodate the ultimate six-lane typical section with frontage roads. The project also includes three new signalized intersections at Meadow Pointe Boulevard, Morris Bridge Road, and US 301.

(1) TITLE AND LOCATION (City and State)

Central Polk Parkway from US 17 (SR 35) to SR 60 PD&E Study, FDOT FTE, Polk County, FL

(2) YEAR COMPLETED

Professional Services 2022

N/A

(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE

Check if project performed with current firm

PD&E Designer. KCA has provided design services for the development of final plans for the new alignment of the Central Polk Parkway from SR 570 (Polk Parkway) to SR 35 (US 17). The proposed roadway will be tolled, and the project includes the design of a gantry for All Electronic Toll Collection. KCA designed an ultimate six-lane divided, limited-access highway typical section with a 74-foot median, 12-foot lanes, and 8-foot inside (4-foot paved) and 12-foot outside shoulders (10-foot paved). Although mostly new construction, the bridges over Landfill Road had to be widened to connect existing bridges to the Polk Parkway. The rest of the alignment for the SR 570 to US 17 section includes a tri-level interchange at Polk Parkway, overpasses at SR 540, Thornhill Road (two locations), Old Bartow Eagle Lake Road, and an at-grade intersection at US 17. The KCA team is responsible for roadway design, structures design, drainage design, traffic/S&PM/lighting/signalization, ITS, TTC, utility coordination/SUE, PD&E re-evaluation, environmental permitting, landscape architecture, tolling architecture, tolling structures, public involvement, surveying/R/W mapping, and geotechnical services.

(1) TITLE AND LOCATION (City and State)

Island Estates Bridges, City of Clearwater, FL

Professional Services
2018

Construction
N/A

(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE

Check if project performed with current firm

Senior Drainage Engineer responsible for drainage design, erosion control, and permitting through SWFWMD. Under the City of Clearwater's Engineer-of-Record (EOR) contract, KCA was tasked with providing engineering services to replace four bridges—Bridge No. 155513: Island Way NB over Clearwater Harbor; Bridge No. 155514: Island Way SB over Clearwater Harbor; Bridge No. 155515: Harbor Passage West Bridge over Clearwater Harbor; and Bridge No. 155516: Harbor Passage East Bridge over Clearwater Harbor—near Clearwater Beach. Services consisted of structural engineering, roadway design, environmental, permitting, drainage, survey, and coastal engineering. KCA also provided structural engineering services to repair the seawall in addition to utility coordination, traffic control plans, bridge inspection, and load ratings.

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E. Resumes of Key Personnel Proposed for this Contract (Complete one Section E for each key person.) 12 NAME 13 ROLE IN THIS CONTRACT 14. YEARS EXPERIENCE b. WITH CURRENT FIRM Gabe Garcia, PE Drainage/Hydraulics Analysis and Plans a. TOTAL 5 21 15. FIRM NAME AND LOCATION (City and State) Kisinger Campo & Associates, Corp., Tampa, FL 16. EDUCATION (Degree and Specialization) 17. CURRENT AND PROFESSIONAL REGISTRATION (State and Discipline) BSCE, University of Florida Professional Engineer: Florida #95445 18. OTHER PROFESSIONAL QUALIFICATIONS (Publications, Organizations, Training, Awards, etc.) Mr. Garcia has over seven years of stormwater engineering experience. He currently serves as a Stormwater Engineer in KCA's Drainage division, Mr. Garcia has experience with software such as AutoCAD, Matlab, GIS, and Bluebeam. 19. RELEVANT PROJECTS (1) TITLE AND LOCATION (City and State) (2) YEAR COMPLETED Professional Services Construction Old Coachman over Alligator Creek - Bridge No. 154252, Pinellas County, FL 2022 Ongoing (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Check if project performed with current firm Drainage Engineer. Under our Pinellas County Miscellaneous Services contract, KCA was tasked to develop a multi-phased preliminary engineering report (PER) for Old Coachman Road over Alligator Creek to determine the condition of the existing bridge and adjacent structures and propose alternatives for design of a bridge replacement and roadway improvements. The bridge is functionally obsolete and has exceeded its design life. It also exhibits considerable deterioration. The initial bridge replacement study (Phase I) investigated whether these structures within an approximate 10-year period should be replaced, rehabilitated. or repaired. The Phase II replacement study, dated July 2012, addressed preliminary geometry, roadway, and structural design to meet the hydraulic needs of the site, including associated permitting, environmental, and construction considerations. Phase IIB included updated hydraulic modelling of the site and surrounding area based on the most recent City of Clearwater data. KCA proceeded with design of the replacement bridge to raise the profile, remove adjacent aging drainage structures, and improve safety. The hydraulic study and environmental permitting of the complex system was completed to minimize impact on the surrounding natural resources. The final design includes a three-span Florida Slab Beam superstructure for function and rapid construction to minimize road closure during construction. (1) TITLE AND LOCATION (City and State) (2) YEAR COMPLETED Professional Services Construction Venetian Boulevard Bridge Replacement, City of St. Petersburg, FL 2024 Ongoing (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Drainage Engineer. KCA was selected under the City of St. Petersburg's Continuing Services Contract to design and support the Venetian Blvd. Bridge Replacement Project in Shore Acres. The existing bridge was functionally obsolete and deteriorating, requiring structural design, permitting, roadway and drainage reconstruction, seawall repairs, and utility coordination. KCA also designed a pedestrian and vehicular bypass for construction detours and incorporated public feedback to minimize community impact, including using non-displacement piles to reduce vibrations. The bridge design was completed in 2024, with construction starting in February 2025. KCA continues to provide post-design services. (2) YEAR COMPLETED Professional Services Construction Central Polk Parkway from US 17 (SR 35) to SR 60 PD&E Study, FDOT FTE, Polk County, FL 2022 N/A (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Check if project performed with current firm Drainage Engineer. KCA has provided design services for the development of final plans for the new alignment of the Central Polk Parkway from SR 570 (Polk Parkway) to SR 35 (US 17). The proposed roadway will be tolled, and the project includes the design of a gantry for All Electronic Toll Collection. KCA designed an ultimate six-lane divided, limited-access highway typical section with a 74-foot median, 12-foot lanes, and 8-foot inside (4-foot paved) and 12-foot outside shoulders (10-foot payed). Although mostly new construction, the bridges over Landfill Road had to be widened to connect existing bridges to the Polk Parkway. The rest of the alignment for the SR 570 to US 17 section includes a tri-level interchange at Polk Parkway, overpasses at SR 540, Thornhill Road (two locations), Old Bartow Eagle Lake Road, and an at-grade intersection at US 17. The KCA team is responsible for roadway design, structures design, drainage design, traffic/S&PM/lighting/signalization, ITS, TTC, utility coordination/SUE, PD&E re-evaluation, environmental permitting, landscape architecture, tolling architecture, tolling structures, public involvement, surveying/R/W mapping, and geotechnical services. (1) TITLE AND LOCATION (City and State) (2) YEAR COMPLETED Professional Services Construction Bimini Drive Bridge Replacement, Monroe County, FL 2023 N/A (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Check if project performed with current firm d. Drainage Engineer. KCA is providing structures, drainage, and roadway design; environmental assessment and permitting; utility coordination; and public involvement services for the Bimini Drive Bridge (Bridge No. 904603) Replacement project. The Bimini Drive Bridge is one of four historic Duck Key bridges,

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requiring evaluation under the National Historic Preservation Act (NHPA).

E. Resumes of Key Personnel Proposed for this Contract (Complete one Section E for each key person.) 12 NAME 13 ROLE IN THIS CONTRACT 14. YEARS EXPERIENCE b. WITH CURRENT FIRM Kellie Loper, PE Constructability/Bidability EOR a. TOTAL 22 22 15. FIRM NAME AND LOCATION (City and State) Kisinger Campo & Associates, Corp., Tampa, FL 16. EDUCATION (Degree and Specialization) 17. CURRENT AND PROFESSIONAL REGISTRATION (State and Discipline) ME, Civil Engineering, University of Florida Professional Engineer in Florida #68417, North Carolina, #041095 BSCE, University of Florida 18. OTHER PROFESSIONAL QUALIFICATIONS (Publications, Organizations, Training, Awards, etc.) Mr. Loper has 22 years of invaluable experience in the CEI industry working in various roles including Inspector Aide, Inspector, Senior Inspector/CSS, Project Engineer, and Senior Project Engineer. His CEI experience includes inspection and oversight of all aspects of asphalt, concrete, earthwork, drainage, structures, and major bridge operations. He is also experienced in CPM schedule reviews, preparation/participation in Disputes Review Board (DRB) hearings, negotiation and claim settlement. Mr. Loper has experience as a Structures Design Engineer with responsibilities including load rating analysis, structure analysis, bridge/structure design, quality control, plans preparation, shop drawing review, cost estimating, and emergency inspection and repair. Mr. Loper also has experience working with KCA's bridge inspection department. As confirming Professional Engineer, he was responsible for signing and sealing all completed Pontis reports and CID records for various structure types such as timber, movable, fixed, fracture critical, and pedestrian overpass bridges, as well as overhead signs and high mast light towers. 19. RELEVANT PROJECTS (1) TITLE AND LOCATION (City and State) (2) YEAR COMPLETED Professional Services Construction Miscellaneous Professional CEI Services, Pinellas County, FL Ongoing Ongoing (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Check if project performed with current firm PM/Senior Project Engineer. KCA is providing CEI services for this contract. Construction tasks include sidewalks, signing and pavement markings, curb and gutter, road reconstruction and widening, stormwater drainage, signalization, water main construction, noisewalls, and structures. Construction Cost: Varies per task work orde (1) TITLE AND LOCATION (City and State) (2) YEAR COMPLETED Construction Professional Services Island Estates Bridges, City of Clearwater, FL 2018 N/A (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Check if project performed with current firm b. Constructability/Bidability Task Lead. Under the City of Clearwater's Engineer-of-Record (EOR) contract, KCA was tasked with providing engineering services to replace four bridges—Bridge No. 155513; Island Way NB over Clearwater Harbor; Bridge No. 155514; Island Way SB over Clearwater Harbor; Bridge No. 155515; Harbor Passage West Bridge over Clearwater Harbor; and Bridge No. 155516: Harbor Passage East Bridge over Clearwater Harbor—near Clearwater Beach. Services consisted of structural engineering, roadway design, environmental, permitting, drainage, survey, and coastal engineering. KCA also provided structural engineering services to repair the seawall in addition to utility coordination, traffic control plans, bridge inspection, and load ratings, (1) TITLE AND LOCATION (City and State) (2) YEAR COMPLETED Professional Services Construction Pasco Countywide CEI Support Services, Pasco County, FL Ongoing Ongoing (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Check if project performed with current firm Senior Project Engineer. This Countywide CEI Support contract is a TWO-based contract. The contract involves providing several Inspectors as needed to provide CEI services for numerous concurrent projects. The individual projects include construction operations such as milling and resurfacing, full depth reclamation, structure and friction course asphalt paving, minor grading, and sodding. The contract also includes management services in the form of review and submittal of Daily Inspections Reports and timesheets. Cost: varies per task order (1) TITLE AND LOCATION (City and State) (2) YEAR COMPLETED Professional Services Construction New Tampa Boulevard Extension over I-75 to Commerce Boulevard, City of Tampa, FL 2012 2012 (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Check if project performed with current firm Bridge Project Engineer/CSS/Senior Bridge Inspector. For this 0.7-mile extension of New Tampa Boulevard to bridge I-75 and connect southward to Commerce Park Boulevard, KCA provided administration and inspection for roadway and bridge construction, including the fabrication and erection of steel girders, provided QC testing for the construction of mechanically stabilized earth (MSE) retaining walls and all structural concrete for the project, reviewed specifications, pay items, and material testing results, performed field measurements and reviewed materials certifications. Project consisted of a 400-footlong, two-span curved steel plate girder bridge, MSE retaining wall, box culvert construction, drainage installation, asphalt paving, pond construction, and other

minor construction. This project was constructed in accordance with all FDOT Specifications and Standards. Construction Cost: \$14 million

	E. Re	sumes of Key P	ersonnel Proposed for this Cone Section E for each key person.)	ontract	KISINGER CAMPO & ASSOCIATES
12. NAME		13. ROLE IN THIS CON	TRACT	14. YEARS EXPERIENCE	
Tom Pride		Environmental Eval	uation/Permitting Task Lead	a. TOTAL	b. WITH CURRENT FIRM
				35	5
	NAME AND LOCATION (City and State)				
Kisinge	r Campo & Associates, Corp., Tampa, FL				
16. EDUC	ATION (Degree and Specialization)		17. CURRENT AND PROFESSIONAL REGIST	RATION (State and Discipline)	
	ogy, Tennessee Technology University logy, Tennessee Technology University				
18. OTHER	R PROFESSIONAL QUALIFICATIONS (Publications, O	rganizations, Training, A	wards, etc.)		
permitti and fres procedu	e's 35 years of experience includes the biologing. His expertise includes ecological assessment water wetland impact analyses and wetland res and he has performed numerous NEPA/Plater supply, and pipeline projects. He recently	ents, habitat and liste mitigation design. H D&E studies and envi	ed species impact analyses, developme le is proficient with federal, state, and lo ronmental permitting for a wide variet	nt of listed species conservation of listed species conservation of listed species conservation.	on measures, coastal teria and permitting
		19.	RELEVANT PROJECTS		
	(1) TITLE AND LOCATION (City and State)			(2) YEAR CO	
	Maydell Drive over Palm River Bridge Repla	acement PD&E/Desi	gn, Hillsborough County, FL.	Professional Services 2021	Construction
(3) BRIEF DESCRIPTION (<i>Brief scope, size, cost, etc.</i>) AND SPECIFIC ROLE Senior Environmental Scientist. This was a highly successful Hills of the existing 616-foot bridge, reconstruction of the approach roa structurally-deficient bridge had been closed to vehicular and pede (630-foot total length) with Florida I-36 Beam superstructure four the sloped abutments for improved scour resistance. The adjacent to accommodate the slightly raised profile. The 10-foot-wide sidew however, this wide sidewalk is designed to eventually accommoda final design plans, specification and construction phase support of SWFWMD Permit, USACE Permits (including a 408 Permit), and a schedule, KCA successfully guided the County through the Statewi coordination approach to assist the contractor to complete constru			adways, drainage improvements, and a destrian traffic for several years. The rejunded on prestressed concrete pile ben't roadway was reconstructed and a corwalk on the bridge connects back to thate the Hillsborough County Trails Mast or this project. Extensive permitting effa Tampa Port Authority Sovereign Subnivide Acceleration Transformation (SWA)	Check if proje LAP project. The project included ditional pedestrian connection placement bridge consisted of ts. Bank and shore rubble riproblemation open/closed drainage existing sidewalks north and er Plan. The KCA Team led all forts were also provided for the nerged Lands Easement. Seek T) process. During construction	vity features. The f six 90-foot spans ap was also provided at ge system was provided d south of the project, aspects of the PD&E study, e USCG Bridge Permit, ing an accelerated design n, KCA led a proactive illion
	(1) TITLE AND LOCATION (City and State) TBNext Segment 2: I-275 (SR 93) from nor North, FDOT District Seven, Pinellas County	th of I-375/5th Aver y, FL	nue N. to north of 38th Avenue	Professional Services Ongoing	Construction N/A
b.	(3) BRIEF DESCRIPTION (<i>Brief scope, size, cost, etc.</i>) AND Chief Scientist. Segment 2 of TBNext (I-275 3 interchanges and 11 bridges. This project's improvements, rigid pavement rehabilitation addressing safety issues (i.e., hydroplaning a and maintaining current bridge clearances a	North) is an Urban Principal Andian of I-275, operational an wall construction. In addition, ive temporary traffic control p	nd lane continuity KCA is evaluating and lan (TTCP), permitting,		
	(1) TITLE AND LOCATION (City and State)			(2) YEAR CO Professional Services	MPLETED Construction
	Central Polk Parkway from US 17 (SR 35) to	SR 60 PD&E Study,	FDOT FTE, Polk County, FL	2022	N/A
	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND	SPECIFIC ROLE		Check if p	project performed with current firm
c.	Senior Environmental Scientist. KCA has provided design services for the development of final plans for SR 570 (Polk Parkway) to SR 35 (US 17). The proposed roadway will be tolled, and the project includes KCA designed an ultimate six-lane divided, limited-access highway typical section with a 74-foot media 12-foot outside shoulders (10-foot paved). Although mostly new construction, the bridges over Landfill to the Polk Parkway. The rest of the alignment for the SR 570 to US 17 section includes a tri-level interchad (two locations), Old Bartow Eagle Lake Road, and an at-grade intersection at US 17. The KCA team drainage design, traffic/S&PM/lighting/signalization, ITS, TTC, utility coordination/SUE, PD&E re-evaluated tolling architecture, tolling structures, public involvement, surveying/R/W mapping, and geotechnical structures.			ne design of a gantry for All El n, 12-foot lanes, and 8-foot ins Road had to be widened to col ange at Polk Parkway, overpas is responsible for roadway de ion, environmental permitting	ectronic Toll Collection. side (4-foot paved) and nnect existing bridges sses at SR 540, Thornhill sign, structures design,
	(1) TITLE AND LOCATION (City and State)	Mine a del C	Lakewa ad Day ab Day al	(2) YEAR CO	
	I-75 Overpass Concept; North Sarasota Mul Sarasota County, FL	iliiilodai Connector,	Lakewood Kanch Development,	Professional Services Ongoing	Construction N/A
	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND	SPECIFIC ROLE		'	project performed with current firm
d.	Chief Scientist. The KCA team provided a provide of I-75 to the west side by constructing Boulevard in Sarasota County near the new rincorporated. The development of this corrid that bypasses University Parkway and providence.	eliminary evaluation a bridge over I-75, co owing lake. The facil lor is important to th	nnecting to North Cattleman Road. The ity will be a four-lane divided roadway e overall transportation network. This o	ension of Lakewood Ranch Bo e project is located just to the with curb and gutter. Bike lan	oulevard from the east south of University es and sidewalks may be

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	E.	Resumes of Key P (Complete o	Personnel Proposed for this Connection Education Education Education Education Education Education (Connection Education Educa	ontract	KISINGER CAMPO	
12. NAME		13. ROLE IN THIS CON	** **	14. YEARS EXPERIENCE	& ASSOCIATES	
Ashley Gorham		Environmental Eva	Environmental Evaluation/Permitting		b. WITH CURRENT FIRM	
				14	8	
15. FIRM 1	NAME AND LOCATION (City and State)			•	•	
Kisinge	r Campo & Associates, Corp., Tampa, FL					
16. EDUC	ATION (Degree and Specialization)		17. CURRENT AND PROFESSIONAL REGIST	RATION (State and Discipline)		
	ogical Restoration, University of Florida ogy, University of Central Florida					
18. OTHER	R PROFESSIONAL QUALIFICATIONS (Publication	s, Organizations, Training, A	Awards, etc.)			
ecosyste such as private o assessm	el-Hadi has 14 years of educational, profes ems of the Southeastern and Western Unit the U.S. Fish and Wildlife Service (USFWS) developers. Her primary responsibilities ind ents, nuisance and/or exotic species mapp g and data collection, environmental perm	ed States. She has work , U.S. Environmental Pr :lude project managem ing, protected species s	ted on a broad range of interdisciplinary otection Agency (EPA), and Bureau of L ent, technical document preparation, ve surveys and relocations, preliminary site	projects in cooperation wit and Management, as well a egetation monitoring, wetlar	h federal agencies s state agencies and nd delineations, habitat	
		19.	RELEVANT PROJECTS			
	(1) TITLE AND LOCATION (City and State)				COMPLETED	
	Maydell Drive over Palm River Bridge Re	eplacement PD&E/Desi	ign, Hillsborough County, FL.	Professional Services 2021	Construction	
	(3) BRIEF DESCRIPTION (Brief scope, size, cost,	,			oject performed with current firm	
a.	Senior Environmental Scientist. This was of the existing 616-foot bridge, reconstructurally-deficient bridge had been clo (630-foot total length) with Florida I-36 Ethe sloped abutments for improved scour to accommodate the slightly raised profil however, this wide sidewalk is designed trinal design plans, specification and const SWFWMD Permit, USACE Permits (includ schedule, KCA successfully guided the Co coordination approach to assist the contra	ction of the approach rosed to vehicular and pe Beam superstructure for resistance. The adjacer e. The 10-foot-wide side o eventually accommoc ruction phase support ing a 408 Permit), and unty through the Statev	padways, drainage improvements, and a destrian traffic for several years. The rej unded on prestressed concrete pile ben'n it roadway was reconstructed and a cor ewalk on the bridge connects back to th late the Hillsborough County Trails Mast for this project. Extensive permitting eff a Tampa Port Authority Sovereign Subn wide Acceleration Transformation (SWA)	additional pedestrian connective placement bridge consisted ts. Bank and shore rubble right in bination open/closed drainge existing sidewalks north a cer Plan. The KCA Team led a corts were also provided for nerged Lands Easement. Set process. During construct ead of schedule. Fees: \$1.6	ctivity features. The of six 90-foot spans prap was also provided at nage system was provided and south of the project, all aspects of the PD&E study, the USCG Bridge Permit, eking an accelerated design tion, KCA led a proactive million	
	(1) TITLE AND LOCATION (City and State)	t City of Ct Datamakee	Fl	(2) YEAR Professional Services	COMPLETED Construction	
	Venetian Boulevard Bridge Replacemen		g, rL	2024g	Ongoing	
b.	(3) BRIEF DESCRIPTION (<i>Brief scope, size, cost, etc.</i>) AND SPECIFIC ROLE Senior Environmental Scientist. KCA was selected under the City of St. Petersburg's Continuing Services Contract to design and support the Ve Bridge Replacement Project in Shore Acres. The existing bridge was functionally obsolete and deteriorating, requiring structural design, permitt and drainage reconstruction, seawall repairs, and utility coordination. KCA also designed a pedestrian and vehicular bypass for construction deteriorated public feedback to minimize community impact, including using non-displacement piles to reduce vibrations. The bridge design w 2024, with construction starting in February 2025. KCA continues to provide post-design services.					
	(1) TITLE AND LOCATION (City and State)			(2) YEAR Professional Services	COMPLETED Construction	
	Central Polk Parkway from US 17 (SR 35)) to SR 60 PD&E Study	, FDOT FTE, Polk County, FL	2022	N/A	
	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) A			Check if project performed with current fir		
C.	Senior Environmental Scientist. KCA has SR 570 (Polk Parkway) to SR 35 (US 17). T KCA designed an ultimate six-lane divider 12-foot outside shoulders (10-foot paved) to the Polk Parkway. The rest of the align Road (two locations), Old Bartow Eagle L drainage design, traffic/S&PM/lighting/sitolling architecture, tolling structures, pub	ne design of a gantry for All n, 12-foot lanes, and 8-foot i Road had to be widened to d ange at Polk Parkway, overp is responsible for roadway d ion, environmental permitti	Electronic Toll Collection. inside (4-foot paved) and connect existing bridges basses at SR 540, Thornhill design, structures design,			
	(1) TITLE AND LOCATION (City and State)				Construction	
	Bimini Drive Bridge Replacement, Monr	oe County, FL		Professional Services 2023	Construction N/A	
	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) A				if project performed with current firm	
d.	Senior Environmental Scientist. KCA is pand public involvement services for the B bridges, requiring evaluation under the N a MOA for all four historic bridges. Tasks i replacement as well as coordination with 404 dredge and fill permit, US Coast Gual permit, and South Florida Water Manager	imini Drive Bridge (Brid ational Historic Preserv nclude assessment of w federal and state resou rd Bridge Permit, Florid	Ige No. 904603) Replacement project. Tation Act (NHPA). KCA led the Section 1 vetland, seagrass, coral, and protected s rce and regulatory agencies. Additional a Department of Environmental Protect	The Bimini Drive Bridge is or 06 (NHPA) process to a succ pecies impacts resulting fron tasks include obtaining US	ne of four historic Duck Key cessful conclusion, obtaining m the proposed structure Army Corps of Engineers	

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E. RESUMES OF KEY PERSONNEL PROPOSED FOR THIS CONTRACT 14. YEARS EXPERIENCE 13. ROLE IN THIS CONTRACT 12. NAME a. TOTAL b. WITH CURRENT FIRM Kenneth Hardin Cultural Resources 45 15. FIRM NAME AND LOCATION (City and State) Janus Research, Inc Tampa, FL. 16. EDUCATION (DEGREE AND SPECIALIZATION) 17. CURRENT PROFESSIONAL REGISTRATION (STATE AND DISCIPLINE) M.A. Cultural Resource Management Register of Professional Archaeologists B.A. Psychology and South Asian Studies Florida Archaeological Council Florida Trust for Historic Preservation 18. OTHER PROFESSIONAL QUALIFICATIONS (Publications, Organizations, Training, Awards, etc.) 19. RELEVANT PROJECTS (2) YEAR COMPLETED (1) TITLE AND LOCATION (City and State) PROFESSIONAL SERVICES CONSTRUCTION (If applicable) MLK/9th Street Bridge 2020 (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE □ Check if project performed with current firm Janus Research worked with the FDOT to conduct a CRAS for the MLK/9th Street Bridge in the Roser Park Historic District within St. Petersburg. This was an FDOT bridge replacement project and Janus Research worked with the Department on all aspects of the Section 106 process. Because the significant bridge was going to be adversely affected by the project, Janus Research assisted with the consultation and the preparation of a Memorandum of Agreement (MOA). Janus Research also implemented the measures outlined in the MOA. Historic American Engineering Record (HAER) documentation was prepared and submitted to the National Park Service when it was finalized. (2) YEAR COMPLETED (1) TITLE AND LOCATION (City and State) PROFESSIONAL SERVICES CONSTRUCTION (If applicable) Bayou Grande Bridge Replacement 2019 (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE □ Check if project performed with current firm b. Janus Research conducted a Cultural Resources Assessment Survey of the Bayou Grande Bridge Replacement project for the City of St. Petersburg. The survey was conducted in support of a U.S. Army Corps of Engineers permit and in compliance with Section 106 of the National Historic Preservation Act. The survey identified and evaluated the significance of five resources, including three historic buildings: the Bayou Grande Bridge, and the Bayou Grande Canal. All were considered National Registerineligible due to the lack of historical associations and non-historic alterations except for the Bayou Grande Bridge which was determined to be eligible for inclusion in the National Register. (2) YEAR COMPLETED (1) TITLE AND LOCATION (City and State) PROFESSIONAL SERVICES CONSTRUCTION (If applicable) Beckett Bridge Evaluation PD&E Study, Tarpon Springs, FL 2012 (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE ☐ Check if project performed with current firm C The historic resources survey resulted in the identification of 16 newly recorded historic resources within the APE. One newly recorded historic resource, the Beckett Bridge (8PI12017), has been determined eligible for listing in the National Register in Pinellas County as an individual historic resource. A Determination of Eligibility and a Section 106 Effects Evaluation were completed for Beckett Bridge were applied to the bridge and it was determined that the No-Build Alternative will result in a no adverse effect finding and the Rehabilitation and Recommended Alternatives, will have an adverse effect on the Beckett Bridge. (2) YEAR COMPLETED (1) TITLE AND LOCATION (City and State) PROFESSIONAL SERVICES CONSTRUCTION (If applicable) Snell Isle Bridge 2023 (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE ☐ Check if project performed with current firm Janus Research worked with KCA on the Snell Isle Bridge in St. Petersburg. Documentation for this project was requested by the Army Corps of Engineers (ACOE) based on a permitting request by the City of St. Petersburg. The CRAS identified six historic resources within the APE. These resources consist of the National Register listed St. Petersburg Woman's Club (8PI580), and the North Shore Historic District (8PI9640). Two of the structures within the APE are listed on the National Register as contributing resources to the North Shore Historic District: 2116 Coffee Pot Boulevard Northeast (8PI3408) and 2130 Coffee Pot Boulevard Northeast (8PI3409). The Snell Isle Bridge (8PI8748) was recommended National Register eligible in 1990 and 2000. However, the resource has not been officially evaluated for National Register eligibility by the Florida State Historic Preservation Officer (SHPO). The Snell

Isle Bridge (8PI8748) is a 1928 concrete tee-beam and slab bridge which was built to carry Snell Isle Boulevard

Northeast over Coffeepot Bayou.

E. RESUMES OF KEY PERSONNEL PROPOSED FOR THIS CONTRACT 14. YEARS EXPERIENCE 12. NAME 13. ROLE IN THIS CONTRACT a. TOTAL b. WITH CURRENT FIRM Kathleen Hoffman Cultural Resources 44 44 15. FIRM NAME AND LOCATION (City and State) Janus Research, Inc Tampa, FL. 16. EDUCATION (DEGREE AND SPECIALIZATION) 17. CURRENT PROFESSIONAL REGISTRATION (STATE AND DISCIPLINE) Ph.D. Anthropology Register of Professional Archaeologists M.A. Anthropology Florida Archaeological Council B.A. Anthropology Florida Trust for Historic Preservation 18. OTHER PROFESSIONAL QUALIFICATIONS (Publications, Organizations, Training, Awards, etc.) 19. RELEVANT PROJECTS (2) YEAR COMPLETED (1) TITLE AND LOCATION (City and State) PROFESSIONAL SERVICES CONSTRUCTION (If applicable) Snell Isle Bridge (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE □ Check if project performed with current firm Janus Research worked with KCA on the Snell Isle Bridge in St. Petersburg. Documentation for this project was requested by the Army Corps of Engineers (ACOE) based on a permitting request by the City of St. Petersburg. The CRAS identified six historic resources within the APE. These resources consist of the National Register listed St. Petersburg Woman's Club (8PI580), and the North Shore Historic District (8PI9640). Two of the structures within the APE are listed on the National Register as contributing resources to the North Shore Historic District: 2116 Coffee Pot Boulevard Northeast (8PI3408) and 2130 Coffee Pot Boulevard Northeast (8PI3409). The Snell Isle Bridge (8PI8748) was recommended National Register eligible in 1990 and 2000. However, the resource has not been officially evaluated for National Register eligibility by the Florida State Historic Preservation Officer (SHPO). The Snell Isle Bridge (8PI8748) is a 1928 concrete tee-beam and slab bridge which was built to carry Snell Isle Boulevard Northeast over Coffeepot Bayou. (2) YEAR COMPLETED (1) TITLE AND LOCATION (City and State) PROFESSIONAL SERVICES CONSTRUCTION (If applicable) Bayou Grande Bridge Replacement 2019 (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE □ Check if project performed with current firm Janus Research conducted a Cultural Resources Assessment Survey of the Bayou Grande Bridge Replacement project for the b. City of St. Petersburg. The survey was conducted in support of a U.S. Army Corps of Engineers permit and in compliance with Section 106 of the National Historic Preservation Act. The survey identified and evaluated the significance of five resources, including three historic buildings: the Bayou Grande Bridge, and the Bayou Grande Canal. All were considered National Registerineligible due to the lack of historical associations and non-historic alterations except for the Bayou Grande Bridge which was determined to be eligible for inclusion in the National Register. (2) YEAR COMPLETED Cultural Resource Assessment Survey for I-275 at Bunces Pass PROFESSIONAL SERVICES CONSTRUCTION (If applicable) Bridge Pinellas County, FL 2011 (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE □ Check if project performed with current firm Janus Research conducted a Cultural Resource Assessment Survey of a two lane low level fixed Interstate 275 southbound bridge over c. Bunces Pass in Pinellas County .Janus Research's study focused on evaluating the potential effects of the project on archaeological resources located within or adjacent to the project corridor. The objective was to determine the potential for unrecorded archaeological sites as well as identify any previously recorded archaeological sites and evaluate their eligibility for listing in the National Register of Historic Places. No evidence of any submerged anomalies or underwater archaeological sites were identified, and the project corridor

(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE

Janus Research worked with the FDOT to conduct a CRAS for the MLK/9th Street Bridge in the Roser Park Historic District within St. Petersburg. This was an FDOT bridge replacement project and Janus Research worked with the Department on all aspects of the Section 106 process. Because the significant bridge was going to be adversely affected by the project, Janus Research assisted with the consultation and the preparation of a Memorandum of Agreement (MOA). Janus Research also implemented the measures outlined in the MOA. Historic American Engineering Record (HAER) documentation was prepared and submitted to the National Park Service when it was finalized.

was considered to have a low probability for archaeological sites. No historic properties were affected by the bridge replacement project.

(2) YEAR COMPLETED

CONSTRUCTION (If applicable)

PROFESSIONAL SERVICES

2020

(1) TITLE AND LOCATION (City and State)

MLK/9th Street Bridge

d.

E. RESUMES OF KEY PERSONNEL PROPOSED FOR THIS CONTRACT 14. YEARS EXPERIENCE 12. NAME 13. ROLE IN THIS CONTRACT a. TOTAL b. WITH CURRENT FIRM Amy Streelman Chief Architectural 27 27 Historian 15. FIRM NAME AND LOCATION (City and State) Janus Research, Inc Tampa, FL. 16. EDUCATION (DEGREE AND SPECIALIZATION) 17. CURRENT PROFESSIONAL REGISTRATION (STATE AND M.H.P. Historic Preservation Florida Trust for Historic Preservation B.A. English National Trust for Historic Preservation 18. OTHER PROFESSIONAL QUALIFICATIONS (Publications, Organizations, Training, Awards, etc.) 19. RELEVANT PROJECTS (2) YEAR COMPLETED (1) TITLE AND LOCATION (City and State) PROFESSIONAL SERVICES SR 686 Roosevelt from N. of Ulmerton Road to E. of 40th Street. CONSTRUCTION (If applicable) Pinellas Park, FL (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE □ Check if project performed with current firm Janus Research conducted a Cultural Resource Assessment Survey for this project which would include archaeological and historic structures background and fieldwork. Archaeological fieldwork will only be required for 3 off-site ponds that have not been previously been surveyed. No FPC sites are anticipated. The general objective of this Supplemental Amendment is to provide services for final plans for an additional break-out project consisting of the SR 686. (2) YEAR COMPLETED (1) TITLE AND LOCATION (City and State) PROFESSIONAL SERVICES CONSTRUCTION (If applicable) MLK/9th Street Bridge 2020 (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE ☐ Check if project performed with current firm b. Janus Research worked with the FDOT to conduct a CRAS for the MLK/9th Street Bridge in the Roser Park Historic District within St. Petersburg. This was an FDOT bridge replacement project and Janus Research worked with the Department on all aspects of the Section 106 process. Because the significant bridge was going to be adversely affected by the project, Janus Research assisted with the consultation and the preparation of a Memorandum of Agreement (MOA). Janus Research also implemented the measures outlined in the MOA. Historic American Engineering Record (HAER) documentation was prepared and submitted to the National Park Service when it was finalized. (2) YEAR COMPLETED (1) TITLE AND LOCATION (City and State) Beckett Bridge Evaluation PD&E Study, Tarpon Springs, FL PROFESSIONAL SERVICES CONSTRUCTION (If applicable) 2012 (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE □ Check if project performed with current firm C. The historic resources survey resulted in the identification of 16 newly recorded historic resources within the APE. One newly recorded historic resource, the Beckett Bridge (8PI12017), has been determined eligible for listing in the National Register in Pinellas County as an individual historic resource. A Determination of Eligibility and a Section 106 Effects Evaluation were completed for Beckett Bridge were applied to the bridge and it was determined that the No-Build Alternative will result in a no adverse effect finding and the Rehabilitation and Recommended Alternatives, will have an adverse effect on the Beckett Bridge. (2) YEAR COMPLETED (1) TITLE AND LOCATION (City and State) PROFESSIONAL SERVICES CONSTRUCTION (If applicable) Snell Isle Bridge 2023 (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE □ Check if project performed with current firm Janus Research worked with KCA on the Snell Isle Bridge in St. Petersburg. Documentation for this project was requested by the Army Corps of Engineers (ACOE) based on a permitting request by the City of St. Petersburg. The CRAS identified six historic resources within the APE. These resources consist of the National Register listed St. Petersburg Woman's Club (8PI580), and the North Shore Historic District (8PI9640). Two of the structures within the APE are listed on the National Register as contributing resources to the North Shore Historic District: 2116 Coffee Pot Boulevard Northeast (8PI3408) and 2130 Coffee Pot Boulevard Northeast (8PI3409). The Snell Isle Bridge (8PI8748) was recommended National Register eligible in 1990 and 2000. However, the resource has not been officially evaluated for National Register eligibility by the Florida State Historic

Preservation Officer (SHPO). The Snell Isle Bridge (8PI8748) is a 1928 concrete tee-beam and slab bridge which was built to

carry Snell Isle Boulevard Northeast over Coffeepot Bayou.

E. R	ESUMES OF KEY PERSONNEL (Complete one Section I		TRACT		
12. NAME		THIS CONTRACT	14. YEARS	EXPERIENCE	
	President, Sen	ior Right of Way	A. TOTAL	B. WITH CURRENT FIRM	
Kaller Laalin		oordinator and			
Kelly Joslin	Litigation	n Support,	26	20	
		agement Agent			
15. FIRM NAME AND LOCATION (City and State)	1 7			1	
ndependence Acquisition & Apprais					
11030 N. US Highway 301, Thonotos	sassa, FL 33592				
6. EDUCATION (Degree and Specialization)		17. CURRENT PROFESSIONA			
B.S., Finance - University of South F	iorida, 1984	Florida Real Estate State-Registered Ti #RI14801; Certified #7ME01127901MP #7ME01107901I00 International Right (rainee Real Estate A Asbestos Manager 004; Certified Asbe 1; Former Florida Li	Appraiser nent Planner stos Inspector, cense Title Agent;	
18. OTHER PROFESSIONAL QUALIFICATIONS (Publication)			•	•	
Real Estate Salesman & Brokers Co		FDOT Advanced R			
Lauderdale, FL; Real Estate Apprais		Federal Highway A		Advanced	
Petersburg, & Miami, FL; Internation		Relocation Case St	udies		
Association 15 IRWA Continuing Edu		T DDOJECTC			
(1) TITLE AND LOCATION (City and State)	19. RELEVAN	I PRUJECTS	(2) YEAR	COMPLETED	
FDOT, District 1: Right of Way Co	onsultant Services Acquis	sition	PROFESSIONAL SERVICES	CONSTRUCTION (If applicable)	
(3) BRIEF DESCRIPTION (Brief scope, size, cost,	etc) AND SPECIFIC ROLE		· ·	formed with current firm	
She has also provided lawsuit pre agent property management serv management responsibilities inclu- inventory updates up to and inclu- Scope: (Administration, Suit Prep	ices on a former full-serv Ided preparing real prope ding final disposition of th	ice Districtwide contra erty/personal property ne property.	ct for District One. H	ler property	
FDOT, District 7: Lochner GEC -	Acquisition and Relocation	on Services	PROFESSIONAL SERVICES √	CONSTRUCTION (If applicable)	
(2) PRIEE DESCRIPTION (Priof scape size sect	ota) AND SPECIFIC DOLE		•	formed with current firm	
(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Ms. Joslin provided R/W staffing on this full-service contract for District Seven. The tasks on this providing relocation and property management agents for the Tampa Presbyterian Village relocation and SR 60 Voluntary Advanced Acquisition and Relocation project with R/W management and staff home office; and Ms. Joslin was tasked to assist the District's Suit Coordinator with suit preparation in and update of title searches/information pertaining to all property interests. Ms. Joslin is responsible updating the title, ensuring the correct parties to the suit are named, making sure all appropriate docu obtained and uploaded into RWMS. Under this task she prepared and provided many lawsuits to Dist Scope: (Acquisition, Relocation, Title Review/Verification, Property Management, Suit Preparation)				s contract includer n project; The I-27 affing from the IAA ncluding the review e for reviewing and cuments have been	
(1) TITLE AND LOCATION (City and State)				COMPLETED	
FDOT, District 7: Sr. Property Ma	nagement Agent (I-275 C	Corridor/Projects)	PROFESSIONAL SERVICES	CONSTRUCTION (If applicable)	
(3) BRIEF DESCRIPTION (Brief scope, size, cost.	etc.) AND SPECIFIC ROLE		v √ Check if project per	formed with current firm	
(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Ms. Joslin served in-house as Senior Property Manager for this project with several hundred parcel acquisitions and multitude of residential and commercial demolitions from downtown Tampa to the Tampa International airport. Her property management responsibilities included the demolition advertising and contracting for over 300 structures. Scope: (Property Management)					

	E. RESU	MES OF KEY PERSONNEL F (Complete one Section E		RACT	
12.	NAME	13. ROLE IN TH		14. YEARS	S EXPERIENCE
Ri	ichard D. Joslin, SR/WA	Manager/Senio	lent/Project or Right of Way elocation Agent	a. total 35	B. WITH CURRENT FIRM
Ind	FIRM NAME AND LOCATION (City and State) dependence Acquisition & Appraisal, I 030 N. US Highway 301, Thonotosas	LLC			
16. EDUCATION (Degree and Specialization) 17. CURRENT PROFESSIONAL REGISTRATION (State and Discipline) 18.A., Business Administration - University of South Florida, 1983 Florida Real Estate Broker #BK0560987; Florida S Registered Trainee Real Estate Appraiser #RI0000 State of Florida Public Notary #GG013016; Senior Member No. 4496, SR/WA International Right of V Association					37; Florida State- iser #RI0003951; 016; Senior
Re La Pe	OTHER PROFESSIONAL QUALIFICATIONS (Publications eal Estate Salesman & Brokers Course auderdale, FL; Real Estate Appraisal Cetersburg, & Miami, FL; International Fesociation 23 IRWA Continuing Educa	es, Tampa & Fort Courses, Tampa, St. Right of Way tion Courses	FDOT Advanced Re Federal Highway Ad Relocation Case Stu	ا Iministration- NHI	
	(1) TITLE AND LOCATION (City and Chata)	19. RELEVANT	T PROJECTS	(2) VEAD	COMPLETED
	(1) TITLE AND LOCATION (City and State) Pinellas County, US 19/SR 55 to 34 th	h Street North 126 th Av	enue (PD&E)	PROFESSIONAL SERVICES	COMPLETED CONSTRUCTION (If applicable)
а.	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) Mr. Joslin managed the CSRP and P Highway 19/SR 55 to 34 th Street Nor residential, industrial, and commercia Scope: (Conceptual Stage Relocation	PD&E study for propose th in Pinellas Park, Flo al properties impacted.	rida. This was a large	g 126 th Avenue No	
b.	FDOT, District 7: R/W Consultant Ac (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) Mr. Joslin is managing this full-service services, on this task oriented continu scope of acquisition services includ management services. Under this co closely with FDOT management. IA to augment District right of way staff.	and specific role of right of way contract uing services contract f ing title update, negot ntract IAA personnel ar A also provided Agent	to provide acquisition, for FDOT District Seve iations, relocation, clo	relocation, and pro n. The responsibili osings, suit prepar equire from the IAA	ties included the ful ation, and property offices coordinating
	(1) TITLE AND LOCATION (City and State) FDOT, District 7: Atkins GEC, Right Program		ion Services – Work	(2) YEAR PROFESSIONAL SERVICES √	COMPLETED CONSTRUCTION (If applicable)
(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Mr. Joslin has been providing right of way cost estimation services for the Department of Transportation, District Seven, for the past 11 years. The scope includes full estimation of many of the Districts projects included in the year work program. Close coordination with the FDOT and Consultant PD&E Engineering Project Managers is required to verify project specifics and identify cost savings opportunities. IAA has produced many estimates an worked closely with the District Cost Estimate Coordinator, Bill McTeer. Scope: (Cost Estimation Services)				ation, District luded in the five- Managers is	
	(1) TITLE AND LOCATION (City and State) FDOT, District 1: R/W Consultant Ac	quisition & Relocation	Continuing Services	(2) YEAR PROFESSIONAL SERVICES √	COMPLETED CONSTRUCTION (If applicable)
SERVICES (If applicable)					pperty managemenies included the ful

	E. RESUMES OF KEY PERSONNEL PROPOSED FOR THIS CONTRACT (Complete one Section E for each key person.)						
12.	NAME	13. ROLE IN TH	IS CONTRACT	14. YEARS	S EXPERIENCE		
	ngolia I. Stovenson	Project Manager, Senior R/W		A. TOTAL	B. WITH CURRENT FIRM		
A	ngelia L. Stevenson	Acquisition & Re	elocation Agent	27	7		
	FIRM NAME AND LOCATION (City and State)						
1	dependence Acquisition & Appraisal, L						
	030 N. US Highway 301, Thonotosass EDUCATION (Degree and Specialization)	sa, FL 33392	17. CURRENT PROFESSIONAL	REGISTRATION (State an	d Discipline)		
_	tended University of South Florida		Florida Real Estate				
Hi	llsborough Community College- AA 19		Florida Notary Publi	c #GG123678			
_	OTHER PROFESSIONAL QUALIFICATIONS (Publications	, Organizations, Training, Awards			2000		
	arious IRWA Professional Courses.	2011	FDOT Supervisors A				
Г	OOT Management Academy Training,	19. RELEVANT	FDOT Right of Way	Training Program	, 2007.		
	(1) TITLE AND LOCATION (City and State)	IJ. KELEVANI	TROJECTO	(2) YEAR	COMPLETED		
	Hillsborough County Real Estate Dep	partment		PROFESSIONAL SERVICES	CONSTRUCTION (If applicable)		
				SERVICES	(п аррпсавіе)		
a.	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.)	AND SPECIFIC ROLE		√ Check if project pe	rformed with current firm		
	Ms. Stevenson provided Sr. Acquisiti						
	Management and Real Estate Servic						
	services including title review and ve			al offers negotiation	ons, preparing		
	agreement for Board approval, em	inent domain suits and	a closings.				
	Sarasota County – FDOT, District 1			PROFESSIONAL	CONSTRUCTION		
				SERVICES	(If applicable)		
	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.)	AND SPECIFIC ROLE		√ Check if project pe	erformed with current firm		
b.	Ms. Stevenson provides project mana						
	Responsibilities include the full scop						
	preparing, and presenting offers, n	egotiations, relocation	closings, preparing e	eminent domain s	suits, and property		
	management services. (1) TITLE AND LOCATION (City and State)			(2) VEAD	COMPLETED		
		PROFESSIONAL	COMPLETED CONSTRUCTION				
	FDOT District 7, Districtwide, Tampa, FL, General Acquisition & Relocation			SERVICES	(If applicable)		
c.				√ Check if project ne	rformed with current firm		
		RIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Stevenson provides Senior Right of Way Agent services for various D7 acqui					
	responsibilities include title review a						
	cost estimates, relocation advisory se				,		
	(1) TITLE AND LOCATION (City and State)				COMPLETED		
	Manatee County, Bradenton, Florida,	63 rd Avenue East		PROFESSIONAL SERVICES	CONSTRUCTION (If applicable)		
d.	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.)				rformed with current firm		
	Ms. Stevenson provides project man						
	Responsibilities include title review a and preparing eminent domain suits.	ind vernication,prepari	ng, and presenting i	niliai oners, negoli	ations, closings		
	(1) TITLE AND LOCATION (City and State)			(2) YEAR	COMPLETED		
	Manatee County, Bradenton, Florida,	75 th Street West		PROFESSIONAL	CONSTRUCTION		
	-			SERVICES	(If applicable)		
	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.)	AND SPECIFIC ROLE		√ Check if project pe	erformed with current firm		
e.	Ms. Stevenson provides project mai		tion services for this				
	Responsibilities include title review	and verification, prepared	aring and presenting				
	acquisition parcels, closings and prepared	paring eminent domain	suits.				

E. RESUMES OF KEY PERSONNEL PROPOSED FOR THIS CONTRACT

(Complete one Section E for each key person.)

12. NAME

Patrick McCormack, PSM

13. ROLE IN THIS CONTRACT

Survey/SUE Manager

14. YEARS EXPERIENCE
a. TOTAL b. WITH CURRENT FIRM

b. WITH CURRENT F

15. FIRM NAME AND LOCATION

2.2 miles long).

Cumbey & Fair, Inc. (Clearwater, FL)



16. EDUCATION (DEGREE AND SPECIALIZATION)

BS/Geomatics, University of Florida 2002

17. CURRENT PROFESSIONAL REGISTRATION

Registered Professional Land Surveyor & Mapper -FL #LS6494

18. OTHER PROFESSIONAL QUALIFICATIONS (Publications, Organizations, Training, Awards, etc.)

19. RELEVANT	PROJECTS				
(1) TITLE AND LOCATION		(2) YEAR COMPLETED			
Innisbrook Forcemain Improvements – PID 003210B Pinellas County, Florida		2017	CONSTRUCTION (if application)		
(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Surveyor of Record - Performed this task under the Pinellas County Su includes Horizontal/Vertical Control, topographic survey, establishing ri and Sketch & Descriptions. Survey limits begin 200' south of Bee Por length approximately 3,225 feet).	rvey, Mapping, SUE & GIS ight-of-way line, SUE Level	Services Contrac "B" Designations,	, SUE Level "A" Test Ho		
(1) TITLE AND LOCATION Ozona Village Offsite Drainage Pinellas County, Florida	PROFES	(2) YEAR (SSIONAL SERVICES 2017	COMPLETED CONSTRUCTION (if application)		
(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Surveyor of Record - Performed this task under the Pinellas County Su includes Horizontal/Vertical Control, topographic survey. Field location locations and verification of all drainage structures, pipe sizes, pi improvements that may fall within the easement including fences, pol corners, block corners, PRMs, and PCPs within subdivisions. Show all Ways, and Parcels.	rvey, Mapping, SUE & GIS n of the existing ditch/swale ipe types and invert elev les, buildings, sheds, etc. I	Services Contrac in Lot 3 & 4 of Pl ations within the Locate/recover, w Easements, Prop	at Book 137, Page 6. Fe survey limits. resider where appropriate, property Lines, Plats, Right		
(1) TITLE AND LOCATION Forest Lake Blvd. Phase II SUE – PID 002110A Pinellas County, FL	PROFES	(2) YEAR (SSIONAL SERVICES 2019	COMPLETED CONSTRUCTION (if application) n/a		
(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Surveyor of Record - Performed this task under the Pinellas County scope includes Quality Level "A" Test Holes for the following locations wall of their vault, Test Holes with trench cuts in the shape of an "X" for additional miscellaneous structures at the County's request. Surved deliverables include points file depicting the VvH locations, Test Hole material, depth, and elevation of the top of utility.	s: 10" RCW, 2" WM, the sic or 1 proposed signal, Test I ey horizontal/vertical locat	le of a (6) 4" Fror Holes and Quality ion for each of Report. Report t	ntier Duct, and the outsity Level "B" Designation the VvH locations. Fit to include pipe size, type		
(1) TITLE AND LOCATION Basin 8 (Smith Bayou) Channel A – PID 001922A Pinellas County, FL	PROFES	(2) YEAR (SSIONAL SERVICES 2021	COMPLETED CONSTRUCTION (if application) n/a		
(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Surveyor of Record - Performed this task under the Pinellas County Survey, Mapping, SUE & GIS Services Contract 156-0014-CN. Limits of Survey: Along the centerline of the channel from west of US Alt 19 to the east side of County Road 1, approximately 2,250 feet in length. Materisting easements and R/W, plat and deeds, and found property corners. Include all surface features within limits including utilities, storm and sanitary sewers, trees 4" DBH and above, driveways, all other structures or buildings within survey limits. Pipe network will be included Deliverables include CADD files and signed/sealed copies of the Topographic Survey in compliance with the latest Pinellas County CADD K for Civil 3D.					
(1) TITLE AND LOCATION Tampa Bay Water: As-Needed Professional Services A Contract for Surveying Services, Pinellas County, Florid	rigi odilionit	(2) YEAR (SSIONAL SERVICES 7/On-going	COMPLETED CONSTRUCTION (if application) n/a		
(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE			ned with current firm		
Surveyor of Record – Provided As-Needed Survey services following a	all Tampa Bay Water Stand	lard Operating Pr			

Eldridge Wilde Wellfield - Locate existing power poles interior roads and provide sketch & description for Duke Energy Easement (Approx.

	E. RESUMES OF KI				CT	
12.	(Complete one Section E for each key person.) 12. NAME 13. ROLE IN THIS CONTRACT 14. YEARS EXPERIENCE					
Joseph R. Antinori, P.E. Senior Geotechnical Engir			er	a. TOTAL		
					18	18
	FIRM NAME AND LOCATION <i>(City and State)</i> Tierra, Inc. , Tampa, Florida					
	EDUCATION (DEGREE AND SPECIALIZATION)		17. CURREN	IT PROFESSIONAL RE	GISTRATIO	ON (STATE AND DISCIPLINE)
	B.S., Civil Engineering			rofessional Engin		·
	OTHER PROFESSIONAL QUALIFICATIONS (Publications, Or	ganizations, Training, Av			-	
Am	nerican Society of Civil Engineers					
Am	nerican Society of Highway Engineers					
		19. RELEVANT P	ROJECTS			
	(1) TITLE AND LOCATION (City and State)				(2) YEAR C	COMPLETED
	Madonna Boulevard Bridge Replacement Pinellas County, Florida			PROFESSIONAL SEF 2021-2024		CONSTRUCTION (If applicable) N/A
a.	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND					ormed with current firm
	Geotechnical Engineer of Record. The project cor will span the Pine Key Cutoff tidal channel and in-	nsisted of replacing the	e existing Ma	adonna Boulevard B	ridge with	a new three span bridge that
	geotechnical input to support the design of the bridge					
	(1) TITLE AND LOCATION (City and State)		(2) YEAR COMPLETED			
	17th Street North Bridge Replacement		PROFESSIONAL SEF	RVICES	CONSTRUCTION (If applicable)	
b.	City of St. Petersburg, Florida		2023		N/A	
IJ.	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Geotechnical Engineer of Record. The project consisted of replacing the 17th Street E			Check if project performed with current firm		
	2 nd Avenue. The new bridge was proposed to be pre					
	support the project.				(0) VEAD (COMPLETED
	(1) TITLE AND LOCATION (City and State) Platt Street Bridge over the Hillsborough River Re	habilitation		PROFESSIONAL SER	· ,	CONSTRUCTION (16 1/2-1/2-1/2-1
	Hillsborough County, Florida			2021	VICES	CONSTRUCTION (If applicable) N/A
C.	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND					ormed with current firm
	Geotechnical Engineer of Record. Tierra performe					
_	Street Bridge over the Hillsborough River Rehabilitation (1) TITLE AND LOCATION (City and State)	on. Joseph provided g	eotecnnicai i	nput to support tne p		provements. Completed
	Indian Rocks Road Bridge Culverts Pinellas County, Florida			PROFESSIONAL SEF	. ,	CONSTRUCTION (If applicable)
				2020-2021		N/A
d.	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE				ormed with current firm	
	Geotechnical Engineer of Record. The project consisted of the preliminary hydrogeological evaluation and analyses associated with two bride culverts and two pipe culverts along Indian Rocks Road South between Cove Drive and Avocado Road in Pinellas County. Tierra's geotechnical					
	study support the preliminary hydrogeological evaluat			na /wooddo /wdd ii	i i iiiolido	County: Florid o gootoomiloar
	(1) TITLE AND LOCATION (City and State) Old Coachman Road over Alligator Creek Bridge Replacement Pinellas County, Florida			(2) YEAR COMPLETED		
				PROFESSIONAL SEF 2017-2018		CONSTRUCTION (If applicable) N/A
	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND	SPECIFIC ROLF		Check if project performed with current firm		
e.	Geotechnical Engineer of Record. The project con		nstruction pl			
	over Alligator Creek in Pinellas County. The design	gn includes permaner	nt retaining	walls along one sid	le of the	bridge abutments and minor
	realignment and/or pavement improvements along the feet in length. Joseph provided geotechnical engine					

replacement.

E. RESUMES OF KEY PERSONNEL PROPOSED FOR THIS CONTRACT (Complete one Section E for each key person.) 12. NAME 13. ROLE IN THIS CONTRACT 14. YEARS EXPERIENCE b. WITH CURRENT FIRM a. TOTAL Marc Novak, Ph.D., P.E. **Geotechnical Engineer** 18 14 15. FIRM NAME AND LOCATION (City and State) Tierra, Inc. (Tampa, Florida) 16. EDUCATION (DEGREE AND SPECIALIZATION) 17. CURRENT PROFESSIONAL REGISTRATION (STATE AND DISCIPLINE) Ph.D., Civil Engineering, University of Florida, 2007 Professional Engineer, Florida No. 67431 M.S., Civil Engineering, University of Florida, 2000 B.S., Civil Engineering, University of Florida, 1999 18. OTHER PROFESSIONAL QUALIFICATIONS (Publications, Organizations, Training, Awards, etc.) **American Society of Highway Engineers** 19. RELEVANT PROJECTS (1) TITLE AND LOCATION (City and State) (2) YEAR COMPLETED SR 679 (Pinellas Bayway) Structure E over Intracoastal Waterway PROFESSIONAL SERVICES CONSTRUCTION (If applicable) **Pinellas County** 2018 - ongoing (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Check if project performed with current firm Senior Geotechnical Engineer. The project consists of the design and construction of a new Structure E Bridge over the Boca Ciego Bay Intracoastal Waterway. The existing Structure E bridge is a bascule bridge which will be demolished and replaced with a new two-lane high level fixed span bridge constructed on a new alignment to the east of the existing bridge. Marc provided geotechnical input for design of the water based piers associated with the new bridge structure. (1) TITLE AND LOCATION (City and State) (2) YEAR COMPLETED Horseshoe Creek Road Bridge Replacement CONSTRUCTION (If applicable) PROFESSIONAL SERVICES Polk County, Florida 2023 Check if project performed with current firm (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Geotechnical Engineer of Record. The project consisted of the design and construction of a new bridge structure to replace the existing Horseshoe Creek Road Bridge. Tierra performed engineering evaluations and analyses to develop geotechnical recommendations for the design and construction of the selected foundation for the new bridge structure. (1) TITLE AND LOCATION (City and State) (2) YEAR COMPLETED Golf Course Road over Gamble Creek Bridge PROFESSIONAL SERVICES CONSTRUCTION (If applicable) Manatee County, Florida 2022 (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Check if project performed with current firm Senior Geotechnical Engineer. The project consisted of replacing the Golf Course Road Bridge located approximately 1,000 feet west of the Golf Course Road and Twin Rivers Trail intersection in Manatee County. Marc evaluated foundation criteria and performed engineering analyses to develop design recommendations for the foundation systems being evaluated by the structural engineer. (1) TITLE AND LOCATION (City and State) (2) YEAR COMPLETED Tampa International Airport Taxiway A and Bridge Structures PROFESSIONAL SERVICES CONSTRUCTION (If applicable) Hillsborough County, Florida 2017 - 2018 N/A Check if project performed with current firm (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Senior Geotechnical Engineer. The project consisted of constructing a new taxiway (Taxiway A) with an associated three span bridge located to the north of an parallel to the existing Taxiway B Bridge. The new bridge will accommodate future North Terminal roadways and service roads. Additionally, the approaches to the new bridge will consist of Mechanically Stabilized Earth walls. Marc provided geotechnical input associated with the design of the proposed Taxiway A bridge foundations and associated MSE and sheet pile walls. (1) TITLE AND LOCATION (City and State) (2) YEAR COMPLETED Ream Wilson Trail Improvements PROFESSIONAL SERVICES CONSTRUCTION (If applicable) Pinellas County, Florida 2017-2018 Check if project performed with current firm (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Senior Geotechnical Engineer. The project consisted of improvements to the Ream Wilson Trail in Pinellas County including the design of a new pedestrian bridge over Alligator Creek and approximately 500 feet of a new concrete sidewalk trail. Approach embankments were to be constructed

at the abutments of the new bridge. Marc evaluated foundation alternatives and performed engineering analyses to develop design

recommendations for the chosen foundation system.

E. RESUMES OF KEY PERSONNEL PROPOSED FOR THIS CONTRACT (Complete one Section E for each key person.)							
12. NAME 13. ROLE IN THIS CONTRACT						14. YEARS EXPERIENCE	
Michael Bair, ASP Chief Scientist			a. TOTAL 28	b. WITH CURREN	IT FIRM		
15. I	FIRM NAME AND LOCATION (City and State)	<u> </u>			20		
	rra, Inc. (Tampa, FL)						
	EDUCATION (DEGREE AND SPECIALIZATION)		17. CURREN	NT PROFESSIONAL RE	EGISTRATI	ON (STATE AND DISCIPLI	NE)
	A., Biology, 1994						
18. (OTHER PROFESSIONAL QUALIFICATIONS (Publications, Org	anizations, Training, Awar	as, etc.)				
		19. RELEVANT P	ROJECTS				
	(1) TITLE AND LOCATION (City and State)	0((0).1		DDOEESSIONAL SE	. ,	CONSTRUCTION (# and	lia a h la \
	Crestmont Avenue, George Boulevard and Colfax Pinellas County, Florida	Street Sidewalks		PROFESSIONAL SEI	RVICES	CONSTRUCTION (If app. N/A	ісавіе)
	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND S					rmed with current firm	
a.	Chief Scientist. Pinellas County is facilitating engi						
	Boulevard and Colfax Street in Clearwater. The proj						
	ramps and driveways along one side of Crestmont evaluation to identify and evaluation known or potent						
	project. Mike evaluated five mainline contamination s				ioa tilat ili	ay and a implementation	ii oi tiic
	(1) TITLE AND LOCATION (City and State)	•				COMPLETED	
	Anclote Road from Anclote Boulevard to Alternat Pinellas County, Florida	e US 19		PROFESSIONAL SE 2020-2022		CONSTRUCTION (If app. N/A	licable)
b.	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND S					rmed with current firm	
٥.	Chief Scientist. Tierra prepared a Level I Contamina						
	and Stormwater improvements project located in Pine						
	within and near the Anclote Road right-of-way and prevaluation report.	esented the findings a	na recomme	endations for addition	nai anaiysi	s in a contamination sc	reening
	(1) TITLE AND LOCATION (City and State)				(2) YEAR	COMPLETED	
	54th Avenue North from 49th Street to 34th Street			PROFESSIONAL SE	RVICES	CONSTRUCTION (If app	licable)
	Pinellas County, Florida		2022 Check if pro		N/A		
c.	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND S		oport to add			rmed with current firm	for the
	Chief Scientist. The County was preparing a Preliminary Engineering Report to address safety concerns and provide complete streets for the Lealman community. The project encompasses 54th Avenue North from 49th Street to 34th Street and is split into west and east segments by the						
	SX railroad crossing. Tierra supported the Preliminary Engineering Report by presenting the findings of a contamination screening which identified						
	and evaluated known or potential contamination sites	within or adjacent to t	he study are	a that may affect im			
	(1) TITLE AND LOCATION (City and State) 42nd Avenue North Roadway and Sidewalk Improv	romonte		PROFESSIONAL SE		COMPLETED CONSTRUCTION (If app.	licable)
	Pinellas County, Florida	rements		2022	TOLO	N/A	reasie)
	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND S	SPECIFIC ROLE		Check if pro	oject perfo	rmed with current firm	
d.	Chief Scientist. Pinellas County was facilitating engineering services for proposed improvements along 42nd Avenue North in St. Petersburg. The						
u.	project included developing plans for the professional engineering work associated with the roadway reconstruction/pavement rehabilitation,						
	construction of sidewalks along both sides of 42nd A						
performed a contamination screening to identify and evaluate known or potential contamination sites within or adjacent to the project limits the affect implementation of the project. Thirteen contamination sites were evaluated and assigned risk ratings and the findings were present							
	contamination tech memorandum to support the project.		aluateu anu	assigned risk rating	ys and the	e illidings were present	leu iii a
	(1) TITLE AND LOCATION (City and State)			(2) YEAR	COMPLETED		
	Lakeview Road and South Keene Road Intersection Pinellas County, Florida	on and Drainage Impr	ovements	PROFESSIONAL SEI	RVICES	CONSTRUCTION (If app.	licable)
	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND S	SPECIFIC ROLE		Check if pro	oject perfo	rmed with current firm	
e.	Chief Scientist. Pinellas County was conducting						
	Improvements. The purpose of the study was to e						
	proposed improvements. Tierra conducted a Level I Contamination Screening Evaluation to identify and evaluate known or potential contamination						
sites within or adjacent to the project area that may affect implementation of the project. Two potential contamination sites were identified w project limits and risk ratings were assigned for each.					Sites were identified Wi	umi me	
,	p system and						

	E. RESUMES OF KE (Comple	Y PERSONNEL PR ete one Section E fo			ACT	
12. NAME 13. ROLE IN THIS CONTRACT 14. YEARS EXPERIENCE						14. YEARS EXPERIENCE
Chris Garth, LEP Senior Scientist			a. TOTAL			
15.	FIRM NAME AND LOCATION (City and State)					
Tie	erra, Inc. (Tampa, FL)					
16.	EDUCATION (DEGREE AND SPECIALIZATION)		17. CURREN	IT PROFESSIONAL RE	EGISTRATIO	ON (STATE AND DISCIPLINE)
B.A	A., Geography, 2001					
18.	OTHER PROFESSIONAL QUALIFICATIONS (Publications, Orga	anizations, Training, Awar	ds, etc.)			
		19. RELEVANT P	ROJECTS			
	(1) TITLE AND LOCATION (City and State)				` ′	COMPLETED
	Sunset Point Road from Kings Highway to Keene Pinellas County, Florida	Road		PROFESSIONAL SEF	RVICES	CONSTRUCTION (If applicable) N/A
2	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND S					rmed with current firm
Senior Environmental Scientist. The County was facilitating engineering services for proposed improvements along Sunset Point Road in Clearwater. Improvements considered included meeting future traffic demands, pedestrian and bicycle connectivity throughout the corridor, safety drainage and roadway improvements for Sunset Point Road from Kings Highway to Keene Road. Chris provided a contamination screening evaluation to support the project.						
	(1) TITLE AND LOCATION (City and State)					COMPLETED
	54th Avenue North from 49th Street to 34th Street Pinellas County, Florida			PROFESSIONAL SEF 2022	RVICES	CONSTRUCTION (If applicable) N/A
b.	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Check if project performed with current firm					
υ.	Senior Environmental Scientist . The County was preparing a Preliminary Engineering Report to address safety concerns and provide complete streets for the Lealman community in Pinellas County. The project encompasses 54th Avenue North from 49th Street to 34th Street and is split into west and east segments by the CSX railroad crossing. Chris provided contamination screening services to support the Preliminary Engineering Report, including recommendations for a supplemental assessment, when necessary.					
	(1) TITLE AND LOCATION (City and State)			· · · · · · · · · · · · · · · · · · ·	COMPLETED	
	126th Avenue North from US 19/SR 55 to 34th Stree Pinellas County, Florida	et North PD&E		PROFESSIONAL SEF 2017-2020		CONSTRUCTION (If applicable) N/A
c.	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND S	PECIFIC ROLE		Check if pro	oject perfo	rmed with current firm
	Environmental Scientist . Chris prepared a Contamination Screening Evaluation Report to support the PD&E study for the proposed improvements along 126th Avenue in Pinellas Park, Florida. The contamination evaluation was performed for the preferred build alternative and thirteen pond alternatives in accordance with Chapter 20 of the FDOT PD&E Manual.					
	(1) TITLE AND LOCATION (City and State)					COMPLETED
	Lakeview Road / South Keene Road Intersection a Pinellas County, Florida	and Drainage Improve	ements	PROFESSIONAL SER 2019-2020		CONSTRUCTION (If applicable) N/A
d.	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Check if project performed with current firm				intersection. Chris provided a n per Chapter 20 of the FDOT	
	(1) TITLE AND LOCATION (City and State) Fort DeSoto Bay Pier and Dock Replacement Pinellas County, Florida			(2) YEAR (COMPLETED	
				PROFESSIONAL SEF 2018-2019		CONSTRUCTION (If applicable) N/A
•	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND S	PECIFIC ROLE		Check if pro	oject perfo	rmed with current firm
e.	Environmental Scientist. Tierra conducted an asbest County, Florida. The survey was requested due to the five (5) paint chip samples were collected for laboral identify, location, condition and approximate quantities.	ne planned demolition atory analysis. The re	of the existir sults of our	ng pier structure. Th survey were provid	nirty-two (3 led to pro	(32) bulk asbestos samples and vide information regarding the

E. RESUMES OF KEY PERSONNEL PROPOSED FOR THIS CONTRACT (Complete one Section E for each key person.)							
12. NAME 13. ROLE IN THIS CONTRACT 14. YEARS EXPERIENCE							
Sandra L. Polanis Geotechnical Field Suppo		port Supervisor	a. TOTAL	b. WITH CURRENT FIRM			
15.	15. FIRM NAME AND LOCATION (City and State)						
	versified Professional Services, Corp., Dad	e City, Florida					
	EDUCATION (DEGREE AND SPECIALIZATION)		RENT PROFESSIONAL R	EGISTRATION	ON (STATE AND DISCIPLINE)		
N/		N/A					
OS #00	18. OTHER PROFESSIONAL QUALIFICATIONS (<i>Publications, Organizations, Training, Awards, etc.</i>) OSHA 29 CFR 1910.120 40 Hour HAZWOPER, OSHA 29 CFR 1910.120 8 Hour Refresher, TSCA Title II Asbestos and Mechanical Inspector #004695, NESHAP Asbestos Building Inspector #326141, Environmental Site Assessor Phase I ESA, FDOT Chapter 22 PD&E, Level 1 and 2 CSER Training						
		19. RELEVANT PROJEC	rs				
(1) TITLE AND LOCATION (City and State) Sarasota Bradenton International Airport				COMPLETED			
	Taxiway Bravo Rehabilitation, Sarasota County	, Florida	PROFESSIONAL SE	ERVICES	CONSTRUCTION (If applicable) N/A		
a.	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND	SPECIFIC ROLE		project perfe	ormed with current firm		
a.	This project included the evaluation and rehabilit responsible for the oversight of the field activitie pavement cores and collection of two (2) CBF recommendations for the project.	es performed by DPS includi	ng three (3) Standar	d Penetra	tion Test (SPT) Borings, 29		
	(1) TITLE AND LOCATION (City and State)			(2) YEAR	R COMPLETED		
	FDOT District I: I-75 Rest Area Charlotte County, Florida		PROFESSIONAL SE	ERVICES	CONSTRUCTION (If applicable) N/A		
b. The proposed project included the construction of two new rest areas (one northbound and one southbound) along I-75. Specific improvements included new rest area buildings with associated canopy structures, new entry and exit ramps, rest area circulation roadways, truck/auto parking areas and storm water ponds to accommodate the proposed I-75 roadway and rest area site drainage. Ms. Polanis was responsible for oversight of subsurface explorations performed by DPS including over 100 auger borings and over 30 Standard Penetration Test (SPT) borings completed to support the design team.							
	(1) TITLE AND LOCATION (City and State)			(2) YEAR	COMPLETED		
	Waterside Preserve – Due Diligence Hillsborough County, Florida		PROFESSIONAL SE	ERVICES	CONSTRUCTION (If applicable) N/A		
c.	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND	SPECIFIC ROLE	Check if p	oroject perfo	ormed with current firm		
The proposed site was being considered for development and construction of seven (7) 4-story apartment buildings and eight (8) townhome structures along with associated paved parking, drives and storm water management ponds. Ms. Polanis was responsible for oversight of subsurface explorations performed by DPS including 42 test pits to depths of approximately 3 to 7.5 feet below existing site grades to support the design team.							
	(1) TITLE AND LOCATION (City and State)		(2) YEAR COMPLETED				
	Arbors at Valhall Pond Hillsborough County, Florida		PROFESSIONAL SE	ERVICES	CONSTRUCTION (If applicable) N/A		
d. (3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE Check if project performed with current firm The project consisted of the design and construction of five (5) multi-family apartment structures, a clubhouse, retention pond and associated pavement areas. Ms. Polanis was responsible for oversight of subsurface explorations performed by DPS including 22 test pits to depths of approximately 5 to 9 feet below existing site grades to support the design team.					etention pond and associated		
	(1) TITLE AND LOCATION (City and State) Central Avenue South Development Pinellas County, Florida			(2) YEAR	COMPLETED		
			PROFESSIONAL SE	ERVICES	CONSTRUCTION (If applicable) N/A		
e.	(3) BRIEF DESCRIPTION (Brief scope, size, cost, etc.) AND SPECIFIC ROLE This project consisted of demolishing an existing building and design and construction of a new 4-story apartment building and associated parking garage, drive areas and underground stormwater treatment/storage facilities. Ms. Polanis was responsible for oversight of subsurface explorations performed by DPS including 5 test pits to depths of approximately 5 to 9 feet below existing site grades to support the design team with recommendations.						

F. EXAMPLE PROJECTS WHICH BEST ILLUSTRATE PROPOSED TEAM'S QUALIFICATIONS FOR THIS CONTRACT

(Present as many projects as requested by the agency, or 10 projects, If not specified.

Complete one Section F for each project.)

20. EXAMPLE PROJECT KEY NUMBER

1

21. TITLE AND LOCATION (City and State)

SR 56 Extension from Meadow Pointe Boulevard to US 301 Design-Build, FDOT District Seven, Pasco County, FL PROFESSIONAL SERVICES 2020

CONSTRUCTION (if Applicable) 2020

23. PROJECT OWNER'S INFORMATION

a. PROJECT OWNER FDOT District Seven b. POINT OF CONTACT NAME Manuel Flores

c. POINT OF CONTACT TELEPHONE NUMBER 813.975.6469

24. BRIEF DESCRIPTION OF PROJECT AND RELEVANCE TO THIS CONTRACT (Include scope, size, and cost)

Cone & Graham Inc. (C&G) in association with KCA, provided all aspects of the creation of the SR 56 Extension from Meadow Pointe Boulevard to US 301 in Pasco County (including providing the right-of-way, PD&E re-evaluation, permitting, utility coordination and relocation, design, drainage, and construction). KCA designed the four-lane divided typical section compatible with the ultimate six-lane section and centered in a 250-foot right-of-way corridor. All ponds were designed and constructed to accommodate the ultimate six-lane typical section with frontage roads. New, single span bridges with barrier protected sidewalks were designed to cross New River. The project also included three new signalized intersections at Meadow Pointe Boulevard, Morris Bridge Road, and US 301.

RFP Relevancy

22. YEAR COMPLETED

Structures Design

Roadway/MOT Design

Drainage Design

Environmental/Permitting

S&PM

Fees: \$4.9 million

Construction: \$58 million



	25. FIRMS FROM SECTION C INVOLVED WITH THIS PROJECT						
a.	(1) FIRM NAME KCA	(2) FIRM LOCATION (City and State) Tampa, FL	(3) ROLE Structures design; roadway design; drainage design; environmental/permitting; S± signalization				
b.	(1) FIRM NAME	(2) FIRM LOCATION (City and State)	(3) ROLE				

F. EXAMPLE PROJECTS WHICH BEST ILLUSTRATE PROPOSED TEAM'S QUALIFICATIONS FOR THIS CONTRACT

(Present as many projects as requested by the agency, or 10 projects, If not specified.

Complete one Section F for each project.)

20. EXAMPLE PROJECT KEY NUMBER

2

21. TITLE AND LOCATION (City and State)

Maydell Drive over Palm River Bridge Replacement PD&E/Design, Hillsborough County, FL 22. YEAR COMPLETED PROFESSIONAL SERVICES CONSTRUC

2021

S CONSTRUCTION (if Applicable)
2023

23. PROJECT OWNER'S INFORMATION

a. PROJECT OWNER
Hillsborough County

b. POINT OF CONTACT NAME Manny Santos, El

c. POINT OF CONTACT TELEPHONE NUMBER 813.307.1921

24. BRIEF DESCRIPTION OF PROJECT AND RELEVANCE TO THIS CONTRACT (Include scope, size, and cost)

This Hillsborough County/FDOT District Seven Local Agency Program (LAP) project consisted of replacing the existing 616-foot bridge, reconstructing the roadway approaches, and providing pedestrian connectivity. This structurally deficient bridge has been closed to vehicular and pedestrian traffic for several years. The replacement bridge consists of six 90-foot spans resulting in a total bridge length of 630 feet. The bridge has 36inch Florida-I Beams, which are supported by pile bents and riprap protection was provided at the sloped abutments. The adjacent roadway was reconstructed and a combination open/closed drainage system was provided to accommodate the slightly raised profile. The 10-foot-wide sidewalk on the bridge connects back to existing sidewalks north and south of the project; however, this wide sidewalk is designed to eventually accommodate the Hillsborough County Trails Master Plan. The KCA team provided a PD&E study, final design plans, specifications, and construction phase support for this project. Extensive permitting efforts were also provided for the USCG Bridge Permit, Southwest Florida Water Management District (SWFWMD) Permit, USACE Permits (including a 408 Permit), and a Tampa Port Authority Sovereign Submerged Lands (SSL) Easement. Seeking an accelerated design schedule, KCA guided the County through the Statewide Acceleration Transformation (SWAT) process.

RFP Relevancy Local municipal client Bridge Replacement Geotechnical Analysis Structural Analysis Roadway Design Permitting Public Involvement

Same key team members

Fees: \$1.59 million

	25. FIRMS FROM SECTION C INVOLVED WITH THIS PROJECT									
	(1) FIRM NAME	(2) FIRM LOCATION (City and State)	(3) ROLE							
а.	KCA	Tampa, FL	Project management; PD&E roadway design/trail design; structures design; S± drainage design; permitting;traffic/ITS/ lighting							
b.	(1) FIRM NAME	(2) FIRM LOCATION (City and State)	(3) ROLE							

F. EXAMPLE PROJECTS WHICH BEST ILLUSTRATE PROPOSED TEAM'S QUALIFICATIONS FOR THIS CONTRACT

(Present as many projects as requested by the agency, or 10 projects, If not specified.

Complete one Section F for each project.)

20. EXAMPLE PROJECT KEY NUMBER

3

21. TITLE AND LOCATION (City and State)

Venetian Boulevard Bridge Replacement, St. Petersburg, FL

PROFESSIONAL SERVICES
Ongoing (Post-design)

CONSTRUCTION (if Applicable)
Ongoing

23. PROJECT OWNER'S INFORMATION

a. PROJECT OWNER City of St. Petersburg b. POINT OF CONTACT NAME Evan Birk, PE

c. POINT OF CONTACT TELEPHONE NUMBER 727.551.3499

24. BRIEF DESCRIPTION OF PROJECT AND RELEVANCE TO THIS CONTRACT (Include scope, size, and cost)

KCA was elected to provide design and post design services for the Venetian Blvd. Bridge Replacement Project under our City of St. Petersburg Continuing Services Contract. The bridge replacement was located in the neighborhood of Shore Acres in a residential area of St. Petersburg. The existing prestressed concrete slab bridge was classified as functionally obsolete with ongoing signs of deterioration. KCA provided structural design and permitting services for the replacement of the bridge as well as the roadway and drainage reconstruction required for the approach roadways. The project also involved seawall reconstruction for portions of the wall residing within the City's right-of-way. KCA was also tasked with designing a pedestrian and vehicular bypass to use as a detour during construction. The existing bridge had major City owned utilities attached and KCA also led the utility design and coordination efforts for this project.

Due to the residential location of this project, KCA assisted the City in early public involvement efforts and incorporated public feedback into the final design. KCA also prioritized designs that reduced impact to the adjacent properties, such as the use of non-displacement piles to mitigate

pile driving vibrations. The final design of the bridge was completed in 2024 with construction commencing February 2025. KCA is currently providing post design services for the City.

RFP Relevancy

22. YEAR COMPLETED

Local project in Pinellas County

Bridge Replacement

Environmental/Permitting

Structures Design

Roadway Design

Drainage Design

Utility Coordination

Same key team members

Fees: \$638,386



	25. FIRMS FROM SECTION C INVOLVED WITH THIS PROJECT							
a.	(1) FIRM NAME KCA	(2) FIRM LOCATION (City and State) Tampa, FL	(3) ROLE Permitting; structures design; roadway design; drainage design; utility design and coordination; TTC; public involvement					
b.	(1) FIRM NAME JANUS	(2) FIRM LOCATION (City and State) Tampa, FL	(3) ROLE Cultural resources					

F. EXAMPLE PROJECTS WHICH BEST ILLUSTRATE PROPOSED TEAM'S QUALIFICATIONS FOR THIS CONTRACT

(Present as many projects as requested by the agency, or 10 projects, If not specified.

Complete one Section F for each project.)

20. EXAMPLE PROJECT KEY NUMBER

4

21. TITLE AND LOCATION (City and State)

Old Coachman Road over Alligator Creek -Bridge No. 154252, Pinellas County, FL PROFESSIONAL SERVICES 2021

CONSTRUCTION (if Applicable)
Ongoing

23. PROJECT OWNER'S INFORMATION

a. PROJECT OWNER Pinellas County

b. POINT OF CONTACT NAME Robert Meador, PE c. POINT OF CONTACT TELEPHONE NUMBER 727.464.8731

24. BRIEF DESCRIPTION OF PROJECT AND RELEVANCE TO THIS CONTRACT (Include scope, size, and cost)

Under our Pinellas County Miscellaneous Services contract, KCA was tasked to develop a multi-phased preliminary engineering report (PER) for Old Coachman Road over Alligator Creek to determine the condition of the existing bridge and adjacent structures and propose alternatives for design of a bridge replacement and roadway improvements. The bridge is functionally obsolete and has exceeded its design life. It also exhibits considerable deterioration. The initial bridge replacement study (Phase I) investigated whether these structures within an approximate 10-year period should be replaced, rehabilitated, or repaired. The Phase II replacement study. dated July 2012, addressed preliminary geometry, roadway, and structural design to meet the hydraulic needs of the site, including associated permitting, environmental, and construction considerations. Phase IIB included updated hydraulic modelling of the site and surrounding area based on the most recent City of Clearwater data. KCA proceeded with design of the replacement bridge to raise the profile, remove adjacent aging drainage structures, and improve safety. The hydraulic study and environmental permitting of the complex system was completed to minimize impact on the surrounding natural resources. The final design includes a three-span Florida Slab Beam superstructure for function and rapid construction to minimize road closure during construction.

RFP Relevancy

Local municipal project

PD&E

22. YEAR COMPLETED

Roadway design

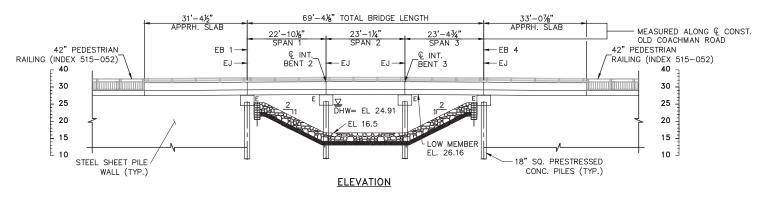
Drainage design

Structures design

Environmental/permitting

Same team members

Fees: \$503,746



Old Coachman Road: plans

	25. FIRMS FROM SECTION C INVOLVED WITH THIS PROJECT								
a.	(1) FIRM NAME KCA	(2) FIRM LOCATION (City and State) Tampa, FL	(3) ROLE PD&E/planning; roadway design; temporary traffic control; S± structures design; public involvement; drainage design						
b.	Tierra	Tampa, FL	Geotechnical/contamination						
C.									

F. EXAMPLE PROJECTS WHICH BEST ILLUSTRATE PROPOSED TEAM'S QUALIFICATIONS FOR THIS CONTRACT

(Present as many projects as requested by the agency, or 10 projects, If not specified.

Complete one Section F for each project.)

20. EXAMPLE PROJECT KEY NUMBER

5

21. TITLE AND LOCATION (City and State)

Madonna Boulevard Bridge Replacement, Pinellas County, FL

PROFESSIONAL SERVICES 2021

CONSTRUCTION (if Applicable)

23. PROJECT OWNER'S INFORMATION

a. PROJECT OWNER Pinellas County

b. POINT OF CONTACT NAME Amin Vosouli, PE c. POINT OF CONTACT TELEPHONE NUMBER 727.464.8889

24. BRIEF DESCRIPTION OF PROJECT AND RELEVANCE TO THIS CONTRACT (Include scope, size, and cost)

KCA was selected by Pinellas County to provide structural design services for the Madonna Boulevard Bridge Replacement project. KCA is providing structural design services for this bridge replacement that provides sole access to the Monte Cristo community in Tierra Verde. A major emphasis of the project was to maintain access for vehicles and pedestrians.

RFP Relevancy

22. YEAR COMPLETED

Local project in Pinellas County

Phase constructed bridge

Roadway design

Drainage design

Structures design

Environmental/permitting

Same key team members

Fees: \$1.125 million



	25. FIRMS FROM SECTION C INVOLVED WITH THIS PROJECT								
a.	(1) FIRM NAME KCA	(2) FIRM LOCATION (City and State) Tampa, FL	(3) ROLE Multi-use trail design; roadway design; TTC; S± structures design; public involvement; drainage design						
b.	Tierra	Tampa, FL	Geotechnical/contamination						
C.	DPS	Tampa, FL	Geotechnical						

	G. Key Personne	el Partio	cipation	in Exai	mple Pr	ojects					
26. NAMES OF KEY PERSONNEL	27. ROLE IN THIS CONTRACT		(Fill in "E)	xample Proje	cts Key" secti	LE PROJECT ion below bef r participation	ore complet	ing table. Pla		project key	
(From Section E, Block IZ)	(From Section E, Block 12) (From Section E, Block 13)		2	3	4	5	6	7	8	9	10
Erin Lawson, PE	PM			✓	✓						
Stephanie Boyd	Deputy PM; Structures Design		✓	✓	✓	√					
Guillermo Madriz, PE	Principal-in-Charge	✓	✓	✓	✓	✓					
Richard Harrison, PE	QA Manager	√	✓	✓	✓	✓					
Deborah Hernandez, PE	Roadway Analysis and Plans EOR	√				✓					
Dean Grumbach, PE	Roaday Analysis and Plans			✓		✓					
Austin, Black, PE	Structures Analysis and Plans	✓	✓	✓		✓					
Dr. Fathy Abdalla, PE, PTOE	S&PM, Lighting Analysis and Plans EOR	✓	✓								
Nick Russin, PE, RSP1	S&PM, Lighting Analysis and Plans										
Tom Pride	Environmental Evaluation/ Permitting Lead		✓								
Ashley Gorham	Environmental Evaluation/ Permitting	✓	✓								
Nicole Selly, ENV SP	Public Involvement Lead		✓	✓							
Diana Albarracin	Public Involvement			✓	✓						
Tara Spieler, PE	Tara Spieler, PE Drainage/Hydraulics Analysis and Plans EOR										
Gabe Garcia, PE	Drainage/Hydraulics Analysis and Plans			✓	✓						
Kellie Loper, PE	Constructability/Bidability										
Kenneth Hardin (Janus)	Cultural Resources			✓							
Kathleen Hoffman (Janus)	Cultural Resources			✓							
Amy Streelman (Janus)	Cultural Resources			✓							
Kelly Joslin (IAA)	R/W Acquisition										
Rick Joslin (IAA)	R/W Acquisition										
Angelia Stevenson (IAA)	R/W Acquisition										
Pat McCormack (C&F)	SUE										
Joseph Antinori, PE (Tierra)	Geotechnical/Contamination				✓	✓					
Dr. Marc Novak, PE (Tierra)	Geotechnical/Contamination				✓	✓					
Michael Bair, ASP (Tierra)	Geotechnical/Contamination				✓	✓					
Sandra Polanis (DPS)	Geotechnical/Contamination					✓					
				ECTS KEY							
No. TITLE OF EXAMPLE PROJ		No.		EXAMPLE I							
SR 56 Extension from Meadow Pointe Boulevard to US 301 Design-Build, FDOT D7, Pasco County		4		achmar s Count		ver Allig	ator Cr	eek - Bri	dge No.	154252,	
	r Palm River Bridge Æ/Design, Hillsborough	5	Madonna Boulevard Bridge Replacement, Pinellas County, FL								
Venetian Bouleva Petersburg, FL	rd Bridge Replacement, St.										

H. Additional Information

1. PROVIDE ANY ADDITIONAL INFORMATION REQUESTED BY THE AGENCY. ATTACH ADDITIONAL SHEETS AS NEEDED.

WHY THE KCA TEAM?

Pinellas County's Selection Committee will no doubt receive numerous proposals from qualified firms. So, why choose KCA? Some highlights/key advantages of our team include the following:

- **>>** KCA's experience with similar projects provides us an in-depth understanding of the requirements of this Pinellas County project. KCA fully understands the economic crisis facing our communities who are struggling to maintain their critical infrastructure
- **>>** KCA's core staff has worked together at our local office in the heart of Downtown Tampa, only 23 miles to the County's office
- >> Successful working relationship with Pinellas County dating back nearly four decades
- >> Proven track record of delivering quality, professional services throughout 48 years of operations in the State of Florida
- >> Established project team with shared experience
- >> Team is led by knowledgeable Project Manager (Erin Lawson, PE) with vast local experience and local resident
- The best corporate support in the industry. The full resources of KCA are at the disposal of the project team to ensure success and client satisfaction
- >> Cost-conscious management
- >> Local staff readily available
- >> No conflicts with existing workload

	I. AUTHORIZED REPRESENTATIVE The foregoing is a statement of facts.	
2. SIGNATURE		3. DATE
		February 27, 2025
4. NAME AND THE		

Guillermo Madriz, PE, Vice President/Principal-in-Charge

Kisinger C 2b. STREET 201 North Fr. 2c-e. CITY, STATE, Tampa, FL 33 6a. POINT OF CON Erin Lawson, 6b. TELEPHONE N 813.871.5331 a. Function Code 02 08 C	NTACT NAME AND TITLE 1, PE, Project Manager NUMBER 60	c. E-MAIL ADDI lawson@k			3. YEAR ESTAB 1976 5a. OWNERSHI Corporatio 5b. SMALL BUS N/A 7. NAME OF FIF N/A 8b. YR. ESTABL N/A	IP TYPE On SINESS STATUS RM (If block 2a	is a branch office) 8c. DUNS
2b. STREET 201 North Fr. 2c-e. CITY, STATE, Tampa, FL 3: 6a. POINT OF CON Erin Lawson, 6b. TELEPHONE N 813.871.5331 a. Function Code 02 08 C	ranklin Street, Suite 900 , ZIP CODE , 3602 NTACT NAME AND TITLE 1, PE, Project Manager NUMBER 9. EMPLOYEES BY DISCIPLINE b. Discipline	lawson@k			5a. OWNERSHI Corporatio 5b. SMALL BUS N/A 7. NAME OF FIF N/A 8b. YR. ESTABL	IP TYPE ON SINESS STATUS RM (If block 2a	is a branch office) 8c. DUNS
201 North Fr. 2c-e. CITY, STATE, Tampa, FL 33 6a. POINT OF CON Erin Lawson, 6b. TELEPHONE N 813.871.5331 a. Function Code 02 08 C	, ZIP CODE 3602 NTACT NAME AND TITLE 1, PE, Project Manager NUMBER 60 E 9. EMPLOYEES BY DISCIPLINE b. Discipline	lawson@k			Corporation 5b. SMALL BUS N/A 7. NAME OF FIF N/A 8b. YR. ESTABL	SINESS STATUS RM (If block 2a	is a branch office) 8c. DUNS
2c-e. CITY, STATE, Tampa, FL 3: 6a. POINT OF CON Erin Lawson, 6b. TELEPHONE N 813.871.5331 a. Function Code 02 A 08 C	, ZIP CODE 3602 NTACT NAME AND TITLE 1, PE, Project Manager NUMBER 60 E 9. EMPLOYEES BY DISCIPLINE b. Discipline	lawson@k			5b. SMALL BUS N/A 7. NAME OF FIF N/A 8b. YR. ESTABL	SINESS STATUS RM (If block 2a	is a branch office) 8c. DUNS
Tampa, FL 33 6a. POINT OF CON Erin Lawson, 6b. TELEPHONE N 813.871.5331 a. Function Code 02 08 C	NTACT NAME AND TITLE I, PE, Project Manager NUMBER 9. EMPLOYEES BY DISCIPLINE b. Discipline	lawson@k			N/A 7. NAME OF FIR N/A 8b. YR. ESTABL	RM (If block 2a	is a branch office) 8c. DUNS
a. Function Code O8 O8 O8	NTACT NAME AND TITLE 1, PE, Project Manager NUMBER 9. EMPLOYEES BY DISCIPLINE b. Discipline	lawson@k			7. NAME OF FIF N/A 8b. YR. ESTABL	LISHED	8c. DUNS
Erin Lawson, 6b. TELEPHONE N 813.871.5331 a. Function Code 02 08 C	9. EMPLOYEES BY DISCIPLINE b. Discipline	lawson@k			N/A 8b. YR. ESTABL	LISHED	8c. DUNS
a. Function Code 02 A 08 C	9. EMPLOYEES BY DISCIPLINE b. Discipline	lawson@k			8b. YR. ESTABL		
a. Function Code 02 A 08 C	9. EMPLOYEES BY DISCIPLINE b. Discipline	lawson@k					
a. Function Code 02 A 08 C	9. EMPLOYEES BY DISCIPLINE b. Discipline		caeng.cor		N/A		/.
Code 02 A 08 C	b. Discipline	c. No. of E					N/A
O2 A 08 C	<u> </u>	c. No. of E		10. PR	OFILE OF FIRM'S EXPERIENCE A REVENUE FOR LAST 5		L AVERAGE
08 0	Administrative	(1) FIRM	mployees (2) BRANCH	a. Profile Code	b. Experience		c. Revenue Inde Number (see below)
	ton motification	32	27	B02	Bridges		8
,	CADD Technician	3	3	E09	Environmental Impact Studies	S	5
14 C	Computer Programmer	6	4	G04	Geographic Information Stud	ies	1
15 C	Construction Inspector	132	18	H07	Highways; Parking Lots		9
16 C	Construction Manager/Engineer	10	2	S13	Stormwater Handling & Facili	ties	7
19 E	Ecologist/Environmental Scientists	12	9	T02	Testing & Inspection Services		8
29 0	GIS	2	2	T03	Traffic & Transportation Engir	neering	6
57 S	Structural Engineer	44	15		Site Design		3
58 T	Technician/Analyst	13	16				
60 T	Transportation Engineer	81	28				
BI E	Bridge Inspector	15					
CBI C	Certified Bridge Inspector	18					
	Total	368	124				
11. ANNUAL	AVERAGE PROFESSIONAL SERVICES REVENUES OF FIR (Insert revenue index number shown at right)			1. Less t		on to less than	
a. Federal Work 1						ion to less than	
b. Non-Federal Work 9					,000 to less than \$1 million 9. \$25 mill	lion to less thar	
c. Total Worl	k)	5. \$1 mil	llion to less than \$2 million 10. \$50 mill	lion or greater		
		I. AUTHORI The foregoing					
2. SIGNATURE	Λ.				3. DATE		
کر 4. NAME AND TIT					February 27, 2025		

Guillermo Madriz, PE, Vice President/Principal-in-Charge

KC:	ARCHITECT - ENGINEER Part II - General Q		1. SOLICITATION NUMBE 25-0196-RFP-CCN				
& ASSOCI	ATES (If a firm has branch offices, complete for each	h specific bra	nch office se	eking work.		4 DUNG NUMBER	
•	BRANCH OFFICE) NAME		3. YEAR ESTABLISHED	4. DUNS NUMBER			
	r Campo & Associates, Corp.		1976	085089126			
2b. STREET	l'. A				5a. OWNERSHIP TYPE		
	gnolia Avenue, Suite 1050				Corporation	THE STATE OF THE S	
	ATE, ZIP CODE				5b. SMALL BUSINESS STAT	IUS	
Orlando, I					N/A	2	
	CONTACT NAME AND TITLE		7. NAME OF FIRM (If block	Za is a branch office			
	son, PE, Project Manager		DECC		N/A	O. DUNG	
5b. TELEPHOI		E-MAIL ADD			8b. YR. ESTABLISHED	8c. DUNS	
813.871.5331 elawson@kcaeng.co					N/A	N/A	
	9. EMPLOYEES BY DISCIPLINE			10. PF	ROFILE OF FIRM'S EXPERIENCE AND ANNU REVENUE FOR LAST 5 YEARS	JAL AVERAGE	
a. Function Code	b. Discipline	c. No. of E (1) FIRM	mployees (2) BRANCH	a. Profile Code	b. Experience	c. Revenue Inde Number (see below)	
02	Administrative	32	2	B02	Bridges	8	
08	CADD Technician	3	0	E09	Environmental Impact Studies	5	
14	Computer Programmer	6	0	G04	Geographic Information Studies	1	
15	Construction Inspector	132	0	H07	Highways; Parking Lots	9	
16	Construction Manager/Engineer	10	1	S13	Stormwater Handling & Facilities	7	
19	Ecologist/Environmental Scientists	12	1	T02	Testing & Inspection Services	8	
29	GIS	2	0	T03	Traffic & Transportation Engineering	6	
57	Structural Engineer	44	4		Site Design	3	
58	Technician/Analyst	13	0				
60	Transportation Engineer	81	16				
BI	Bridge Inspector	15	0				
CBI	Certified Bridge Inspector	18	0				
	Total	368	24				
	UAL AVERAGE PROFESSIONAL SERVICES REVENUES OF FIR (Insert revenue index number shown at right)			1. Less t	DNAL SERVICES REVENUE INDEX NUMBER han \$100,000. 6. \$2 million to less th 00 to less than \$250,000 7. \$5 million to less th		
a. Federa		1		,000 to less than \$500,000 8. \$10 million to less th			
	ederal Work	9	4. \$500,000 to less than \$1 million 9. \$25 million to less than \$50 million				
c. Total W	/ork	9	9	5. \$1 mi	llion to less than \$2 million 10. \$50 million or great	er	
			ZED REPRE g is a stater				
2. SIGNATURI					3. DATE		
					February 27, 2025		
1. NAME AND	OTITLE V						

Guillermo Madriz, PE, Vice President/Principal-in-Charge

1. SOLICITATION NUMBER (If any) **ARCHITECT – ENGINEER QUALIFICATIONS** 25-0196-RFP-CCNA **PART II - GENERAL QUALIFICATIONS** (If a firm has branch offices, complete for each specific branch office seeking work.) 2a. FIRM (OR BRANCH OFFICE) NAME 3. YR ESTABLISHED 1975 067210872 Cumbey & Fair, Inc. 2463 Enterprise Road Corporation FL 33763 Clearwater b. SMALL BUSINESS STATUS 6a. POINT OF CONTACT NAME AND TITLE SBE Certified w/Pinellas County Patrick McCormack, PSM / Vice President 7. NAME OF FIRM (If block 2a is a branch office) n/a (727) 797-8982 pmccormack@cumbeyfair.com 8a. FORMER FIRM NAME(S) (If anv) 8b YR. ESTABLISHED 8c. UNIQUE ENTITY IDENTIFIER N/A 9. EMPLOYEES BY DISCIPLINE 10. PROFILE OF FIRM'S EXPERIENCE AND ANNUAL AVERAGE REVENUE FOR LAST 5 YEARS a Functio c. No. of Employees a. Profile b. Discipline b. Experience Number (1) FIRM) BRANCE 02 Administration 2 C16 Construction Surveying 1 38 Land Surveyor 3 D04 Design Build - Prep for RFP 80 **CADD Technician** 6 G03 Geodetic Survey - Grind & Airborne 1 Highways; Streets; Airfield Paving; Parking Lots 12 Civil Engineer 4 H07 4 60 Transportation Engineer 1 H13 Hydrographic Surveying 48 Project Manager 1 104 Intelligent Transportation Systems 2 13 2 Survey and SUE Crew Irrigation; Drainage 106 _02 Land Surveying 4 _06 Lighting (Exteriors, Streets, Memorials, Athletic Fields) 2 S04 Sewage Collection, Treatment & Disposal 2 S10 Surveying, Planning, Mapping 4 2 S13 Stormwater Handling & Facilities T03 Traffic and Transportation Engineering 4 T04 Topographic Surveying & Mapping 4 W03 Water Supply; Treatment & Distribution 2 Other Employees: Total 30 11. ANNUAL AVERAGE PROFESSIONAL SERVICES PROFESSIONAL SERVICES REVENUE INDEX NUM BER **REVENUES OF FIRM FOR LAST 3 YEARS** (Insert revenue index number shown at right) 1. Less than \$100,000 6. \$2 million to less than \$5 million 2. \$100,000 to less than \$250,000 7. \$5 million to less than \$10 million a. Federal Work 3. \$250,000 to less than \$500,000 8. \$10 million to less than \$25 million b. Non-Federal Work 6 4. \$500,000 to less than \$1 million 9. \$25 million to less than \$50 million 5. \$1 million to less than \$2 million 10. \$50 million or greater 6 c. Total Work 12. AUTHORIZED REPRESENTATIVE The foregoing is a statement of facts b. DATE a. SIGNATURE February 5, 2025 c. NAME AND TITLE Patrick McCormack, PSM / Vice President

ARCHITECT ENGINEER QUALIFICATIONS

1. SOLICITATION NUMBER (If any)

PART II - GENERAL QUALIFICATIONS

(If a firm has branch offices, complete for each specific branch office seeking work.)

2a. FIRM (OR BRANCH OFFICE) NAME Tierra, Inc.										3. YEAR ESTABLISHED 1992 4. UNIQUE ENTITY IDENTIFIER KJS8GUCMSLP3				
2b. STREET									5. OWNERSHIP					
7351 Templ	e Terrace I	Highway							a. TYPE					
2c. CITY					2d. STAT		2e. ZIP		Corpo	ration				
Tampa					FL		3363		b. SMAL	L BUSINESS STA	TUS			
6a. POINT OF 0	CONTACT NAM	ME AND TIT	LE											
Larry P. Mo	ore, P.E., V	ice Presi	dent						7. NAME	OF FIRM (If block	2a is a branch offic	e)		
6b. TELEPHON	E NUMBER		6c. E-MAIL ADDI	RESS					1					
(813) 989-13	354		Imoore@tier	raeng.com										
, ,		8a. FO	RMER FIRM NA	ME(S) (If any)					8b. YR. ESTAB	LISHED	8c. DUNS NU	MBER		
	9. EM	IPLOYEE	S BY DISCIPLI	NE			ANI			OF FIRM'S EX SE REVENUE				
				c. No. of Em	plovees							c. Revenue		
a. Function Code		b. Discipli	ne	(1) FIRM (2) BRANCH		a. Profile Code		b. Experience			Index Number (see below)			
2	Administra	ative			S)5	Soils	& Geolo	gic Studies: Fo	oundations	7			
8	CADD Ted	chnician	cian 3)2	Testir	ng and li	nspection Serv	ices	6		
15	Constructi			98										
24	Environme			6										
27	Foundatio		nnical Eng	31										
58	Techniciar			29										
	Other (En	gineering	Interns)	11										
Total '														
11 ANNII	AI AVFRA	GE PROF		199	PROF	FESSI	ONAI	SFRV	/ICES R	EVENUE INDE	X NUMBER			
					than \$100			- 0 - 1 ()	6.	\$2 million to I		nillion		
FOR LAST 3 YEARS 2. \$100,000 to I								0,000	7.	\$5 million to I				
(Insert revenue index number shown at right) 3. \$250,000 to I									8.	\$10 million to	,			
a. Federal W	/ork				,000 to les				9.	\$25 million to) million		
b. Non-Federal Work 8 5. \$1 million to le					illion to le	ss tha	n \$2 n	nillion	10.	\$50 million or	r greater			
c. Total Work			8											
				12. AUTH	HORIZED R									

Larry P. Moore, P.E., Vice President	
c. NAME AND TITLE	
Lawy Wood	February 6, 2025
a. SIGNATURE	b. DATE

ARCHITECT ENGINEER QUALIFICATIONS

1. SOLICITATION NUMBER (If any)

PART II - GENERAL QUALIFICATIONS

(If a firm has branch offices, complete for each specific branch office seeking work.)

2a. FIRM (OR	BRANCH OFFICE) NAME	3. YEAR ESTABLISHED	4. D	UNS NUMBER					
Diversifie	d Professional Serv	2007 87-658-9370							
2b. STREET	5. OWNERSHIP								
27915 John	ston Road	a. TYPE							
2c. CITY		Corporation							
Dade City				Florida	33523		b. SMALL BUSINESS STATU	JS	
6a. POINT OF	CONTACT NAME AND TITLE	E					SBE/WMBE		
Sandra L. P	olanis						7. NAME OF FIRM (If block 2a	ı is a brı	anch office)
President									
6b. TELEPHOI	-	6c. E-MAIL ADDI							
352.588.281	1	spolanis@DP	S-Corp.con	n					
	8a. F	ORMER FIRM I	NAME(S) (If	any)			8b. YR. ESTABLISHED	8c.	DUNS NUMBER
								<u></u>	
9. EMPLOYEES BY DISCIPLINE							FILE OF FIRM'S EXPER ERAGE REVENUE FOR	_	
	c. No. of Employees				LIVAGE REVENUE FOR	LAO	c. Revenue Index		
a. Function Code	b. Disciplii	b. Discipline		a. Profile Code	b. Experience		Number		
			(1) FIRM	(2) BRANCH					(see below)
2	Administrative		2		S05		Geologic Studies: Foundation	ons	4
8	CADD Technician		1		T02		and Inspection Services		2
58	Technician		6		E12	Env	Environmental Remediation 6		
24	Environmental Scientis	t	2						
30	Geologist		1						
12	Civil Engineer		1						
								\longrightarrow	
-									
		Total	13						

11. ANNUAL AVERAGE PROFESSIONAL SERVICES REVENUES OF FIRM FOR LAST 3 YEARS

(insert revenue index number snown at right)							
a. Federal Work	3						
b. Non-Federal Work	6						
c. Total Work	6						

PROFESSIONAL SERVICES REVENUE INDEX NUMBER

Less than \$100,000. 1.

- \$100,00 to less than \$250,000
- \$250,000 to less than \$500,000
- 4. \$500.000 to less than \$1 million
 - \$1 million to less than \$2 million
- \$2 million to less than \$5 million
- 7. \$5 million to less than \$10 million
- \$10 million to less than \$25 million \$25 million to less than \$50 million
- \$50 million or greater

12. AUTHORIZED REPRESENTATIVE

The foregoing is a statement of facts.

b. DATE

February 6, 2025

c. NAME AND TITLE

a. SIGNATURE

Sandra L. Polanis, President

ARCHITECT ENGINEER QUALIFICATIONS

1. SOLICITATION NUMBER (If any) 190-0068-NC (SS)

			has	branch off	fices, comp	olete for eac	h specif	ïc branch (office seeking v				
2a. FIRM (OR BRANCH OFFICE) NAME Janus Research, Inc.						•		3. YEAR ESTABLIS	3. YEAR ESTABLISHED 4. DUNS NUMBER 1979 055004881			R	
2b. STREET									5.	. OWNE	RSHIP		
1107 N. ¹	Ward Stre	et							a. TYPE				
2c. CITY						2d. STATE	2e. ZII	CODE	Corporati	on			
Tampa					FL 33607 b. SMALL BU			b. SMALL BUSINE					
6a. POINT OF C	ONTACT NAM	E AND TI	TLE						Small Bu	siness			
Kenneth Hardin, President 6b. TELEPHONE NUMBER 6c. E-MAIL ADDR			7500				7. NAME OF FIRM	l (If block 2	a is a brand	ch office))		
							l						
· · · · · · · · · · · · · · · · · · ·					us-researc	n.com			1				
		8a.	FOR	MER FIRM N	IAME(S) (If a	ny)			8b. YR. ESTABL	ISHED	8c. DUN	IS NUM	BER
Piper Arc	haeology								1979		05500)4881	
	9. EM	IPLOYE	ESI	BY DISCIPL	INE		AN			LE OF FIRM'S EXPERIENCE AND RAGE REVENUE FOR LAST 5 YEARS			
a. Function				c. No. of	Employees	a. Profile				c. Re	c. Revenue Index		
Code	n Discipline		pline				Code		b. Experience		Number (see below)		
05	Archaeologi	st			14	(2) BRANCH	H08	Historia	cal Preservation		5		<u>''</u>
06	Architectural I				4		E01		ological Investigations		+	5	
02	Administrative	1			4								
08	CADD Techni	cian			1								
29	GIS Specialist				1						+		├──
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	Other Emp	loyees											
				Total	24								
	AL AVERAG					PROFES	SSIONAL	SERVICES	S REVENUE INC	DEX NUN	ЛBER		
SERV	ICES REVE			RM	1. Less than \$100,000. 6. \$2 million to less than \$5 million								
(Insert reve				at right)	2. \$100	,000 to less tha	an \$250,0	00	7. \$5 million to	o less thai	า \$10 mill [:]	ion	
(Insert revenue index number shown at right)				1	1	,000 to less the			8. \$10 million				
a. Federal W			0			,000 to less the			 \$25 million \$50 million 			llion	
b. Non-Federal Work 6			J. Ψ1111	illion to less th	απ ψ ∠ πππ	OH	το. ψου πιιιιοπ	or greater					
c. Total Wor	r.		6		40.4								
					_	ORIZED REP oing is a stat	_						
a. SIGNATURE	Keem	WH	an							b. DATE Febr	uary 4,	2025	
c. NAME AND T	ITLE	<i>b</i> (-		
Kenneth I	Hardin, Pres	ident											

PART II - GENERAL QUALIFICATIONS

STANDARD FORM 330 (REV. 8/2016) **PAGE 6**

02. Statements and Documentation

CA is a Tampa Bay area engineering consultant with 48 years of experience in a wide variety of civil infrastructure projects including PD&E studies and structures, transportation, stormwater, and environmental projects. KCA has a staff of nearly 380 including 99 PEs who regularly provide design, document preparation, planning, permitting, and construction inspection and management services to a wide variety of municipal and private clients.

KCA OFFICE LOCATIONS

KCA has several offices throughout Florida including Tampa, Brandon, Ft. Myers, Miami, Tallahassee, and Orlando.

Work for this contract will be performed from KCA's Tampa office, our corporate headquarters. All the proposed staff including our PM work in this office in downtown Tampa. This office is only half an hour away from Pinellas County's offices. This close proximity will allow for effective assignment performance. See the table to the right for contact information for KCA and our proposed subconsultants.

REQUIRED DOCUMENTATION

As requested in Pinellas County's RFP, KCA has included the following documents in this section:

- Licenses and certifications for key personnel
- State of Florida business license
- State of Florida corporate registration
- MBE and SBE Certificates
- Attachment A: SBE Status Form

Contact Information



Erin Lawson, PE, PM

201 N. Franklin Street, Suite 900, Tampa, FL 33602

4 813.871.5331

□ ELawson@kcaeng.com



Pat McCormack, PSM

2463 Enterprise Road, Clearwater, FL 33763

**** 727.797.8982

(pmccormack@cumbeyfair.com



Sandra Polanis

27915 Johnston Road, Dade City, FL 33523

352.588.2811



Rick Joslin, SR/WA

9 11030 US Highway 301, Thonotosassa, FL 33592

& 813.773.5400

☐rjoslin@iaallc.com



Amy Streelman

• 1107 N Ward Street, Tampa, FL 33607

**** 813.636.8200 |

◯ amy_streelman@janus-research.com



Larry Moore, PE

7351 Temple Terrace Highway, Tampa, FL 33637

4 813.989.1354

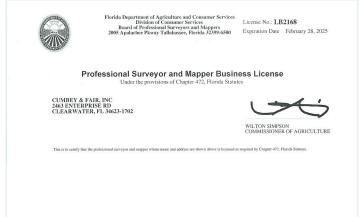
☑Imoore@tierraeng.com

Firm Licenses

KCA



C&F



DPS



Tierra



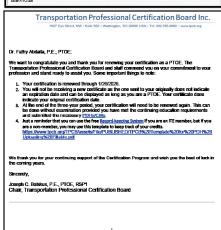
Personnel Professional Licenses

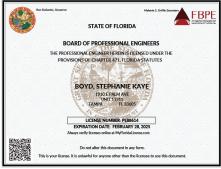










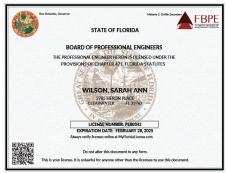






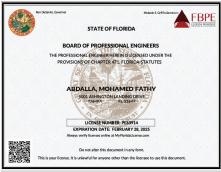














Personnel Professional Licenses





















SBE and MBE Certificates

12/7/2022 SIGNED.

PINELLAS COUNTY SMALL BUSINESS ENTERPRISE PROGRAM THIS CERTIFICATE IS AWARDED TO Cumbey & Fair, Inc. HAS SUCCESSFULLY COMPLETED THE SBE Certification Requirements for: Construction Engineering Inspection, Construction Management, Construction Surveying & Consulting Engineering Certification Expires: 12/6/2025







RIFP# RIFP Title

SMALL BUSINESS ENTERPRISE (SBE) STATUS FORM

SMALL BUSINESS ENTERPRISE (SBE) STATUS FORM

IMPORTANT:

- 1. There is a maximum of 100 points available under this section, which will be awarded as follows:
 - If the prime firm is certified as a Pinellas County SBE, 100 points will be awarded.
 - b. If the prime firm utilizes 1 certified Pinetas County SBE as sub-consultant, 50 points will be awarded.
 - If the prime firm utilizes more than 1 certified Pineths County SBE, as sub-consultant, 75 points will be excerded.
 - d. If the prime firm nor any of its sub consultants are not certified as a Pinellas County SBE, 0% of the points available will be awarded.
- Proof of certification for each firm claiming Pinelles County SBE status should be included in the submittel.

	PINELLAS COUNTY CERTIFIED SBE				
PRIME FIRM	Yes	No			
Kisinger Campo & Associates, Corp. (KCA)					
	PINELLAS COUNT SBE	Y CERTIFIED			
SUB-CONSULTANT(S):	Yes	No			
Cumbey & Fair, Inc.	•				
Diversified Professional Services, Inc.	■ FL MBE				
Independence Acquisition & Appraisal, LLC					
Janus Research, Inc.		•			
Tierra, Inc.		•			

I certify that the information included in this Form is true and complete to the best of my knowledge and belief. I further understand and agree points awarded to this section will be based on the information provided and that this Form shall become a part of my contract with Pinelius County.

Name and Tile of Authorized Representative Guillermo Madriz, PE, Vice President

AVAILABLE POINTS	100 Points (Prime Firm is Pinellus County	AWAR					
MAXIMUM	E	OR PINELLAS COUN					

03. Certificate of Insurance

CERTIFICATE I BELOW. THIS REPRESENTAT IMPORTANT: If If SUBROGATIO this certificate PRODUCER	OES NOT AFFIRMATIVI ERTIFICATE OF INSUR VE OR PRODUCER, AN			ICATE OF LIA					09	/23/2024
If SUBROGATION this certificate RODUCER		ELY O	R NE	NFORMATION ONLY AND GATIVELY AMEND, EXTEI ES NOT CONSTITUTE A C RTIFICATE HOLDER.	ND OR A	LTER THE C	OVERAGE A	AFFORDED BY THE POL	ICIES	i
	N IS WAIVED, subject t	o the	terms	ONAL INSURED, the polic and conditions of the po cate holder in lieu of such	licy, cer h endors	tain policies sement(s).				
Prown & Prown Inc					CONTAC NAME:	^T Vicky Van				
	rance Services, Inc.				PHONE (A/C, No,	Ext): (727) 46		FAX (A/C, No):	(727) 4	142-7695
40 Fountain Parkv uite 600	ay N				E-MAIL ADDRES		Wormer@bbro			
it. Petersburg				FL 33716	INCUIDED	TI 01		RDING COVERAGE Insurance Company		NAIC # 25615
NO.				INSURER B: The Charter Oak Fire Insurance Company INSURER B: The Travelers Indemnity Company of America					25666	
	nger Campo & Associates,	Corp			INSURER	· - ·		ualty Company of America		25674
201	N Franklin St, Suite 400				INSURER D: Travelers Casualty and Surety Company					19038
					INSURER	A design to the	nsurance Com	pany		24856
Tan	ра			FL 33602	INSURER	RF:				
OVERAGES				NUMBER: 24-25				REVISION NUMBER:		
INDICATED. NOT CERTIFICATE MA EXCLUSIONS AN	VITHSTANDING ANY REQU ' BE ISSUED OR MAY PERT	IREME AIN, T OLICIE	ENT, TE HE INS S. LIM	ELISTED BELOW HAVE BEEN ERM OR CONDITION OF ANY SURANCE AFFORDED BY THE ITS SHOWN MAY HAVE BEEN	CONTRA E POLICIE N REDUCE	CT OR OTHER ES DESCRIBEI ED BY PAID CL	R DOCUMENT V D HEREIN IS S AIMS.	WITH RESPECT TO WHICH T	ΓHIS	
	E OF INSURANCE	ADDI INSD	SUBR WVD	POLICY NUMBER		POLICY EFF (MM/DD/YYYY)	POLICY EXP (MM/DD/YYYY)	LIMI		
COMMERC	AL GENERAL LIABILITY							EACH OCCURRENCE	φ	0,000
CLAIN	S-MADE X OCCUR							DAMAGE TO RENTED PREMISES (Ea occurrence)	\$ 300,	
.				.	40/04/0004	10/01/0005	MED EXP (Any one person)	\$ 10,0		
`		.		P-630-8254A604-COF-24	FERSONAL & ADV INSURT		Ψ	0,000		
	TE LIMIT APPLIES PER:							GENERAL AGGREGATE \$ 2,000,00		
POLICY	PRO- JECT LOC							PRODUCTS - COMP/OP AGG	Ψ	
AUTOMOBILE L	A DILLITY	+	-					Employee Benefits COMBINED SINGLE LIMIT	\$ 1,00 \$ 1,00	
-	ABILITY							(Ea accident)		0,000
ANY AUTO OWNED	SCHEDULED			810-5N338364-24-43-G		10/01/2024	10/01/2025	BODILY INJURY (Per person) BODILY INJURY (Per accident)		
AUTOS ON HIRED	AUTOS ONLY AUTOS NON-OWNED OF STREET		010 01000000 24 40 0	10/01/2024	10/01/2023	PROPERTY DAMAGE	\$			
AUTOS ON	Y AUTOS ONLY							(Per accident) \$ Uninsured motorist \$ 1,000,0		0.000
✓ UMBRELLA	LIAB X OCCUR							EACH OCCURRENCE		0,000
C EXCESS LI	- CCCOR			CUP-7J748484-24-43		10/01/2024	10/01/2025	AGGREGATE	¢ 4,00	0,000
DED X	RETENTION \$ 10,000	1						AGGILGATE	\$	
WORKERS COM	ENSATION							➤ PER OTH-ER	Ť	
AND EMPLOYER ANY PROPRIETO	R/PARTNER/EXECUTIVE I	ı İ		LID 007 1070200	10/00/0004			E.L. EACH ACCIDENT	s 500,	000
OFFICER/MEMBE (Mandatory in NE		N/A		UB-007J070308		10/03/2024 10/03/	10/03/2023	E.L. DISEASE - EA EMPLOYEE	\$ 500,	000
If yes, describe un DESCRIPTION O	er OPERATIONS below	\perp						E.L. DISEASE - POLICY LIMIT	\$ 500,	000
	iability - Architects &							Per Claim	5,00	0,000
E Engineers	,			EO000027205-10		10/01/2024	10/01/2025	Aggregate		0,000
				01, Additional Remarks Schedule,				Deductible	250,	000

04. Key Personnel Statement

he KCA team has been carefully structured to provide exceptional service and experienced staff under this contract. Please see Tab 6 for a summary of our team's qualifications. We also realize flexibility throughout a project is important. Flexibility is essential when unforeseen circumstances occur. For this reason we have backup staff available for key KCA team staffing positions to ensure that County expectations are met without sacrificing schedule.

One of the most important considerations in evaluating workload and ability of the KCA team to handle the scope of services is comparing the specific expertise the County needs to that of the KCA team.

ABILITY TO ADDRESS EACH ASPECT OF THE SCOPE OF SERVICES

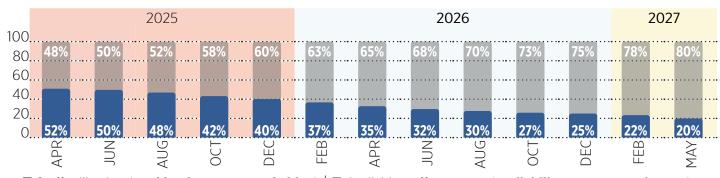
This contract will require a diverse range of technical disciplines. The KCA team possesses these requisite skills and has provided similar services to several local clients under previous and ongoing contracts, including Pinellas County. We have assembled a strong, multidisciplinary team fully qualified to perform the scope and address all design needs. Our team is committed to providing the County with outstanding services through effective, practical, constructible, and compliant design. We have vast resources, including 378 professionals, to supplement this contract asneeded as well as the in-depth expertise/knowledge. skills, and location to serve the County promptly with a wide range of services. KCA directs all projects based upon a carefully thought out Project Management Plan, scope of work, and work plan. The Project Management Plan will summarize pertinent project data, personnel, timelines, milestones, critical path items, costs, progress/ status reporting, and conflict resolution procedures.

The KCA team takes great pride in our history of providing quality, timely services for our clients. We strive toward developing improvements for project delivery and cost savings for our clients, and we are confident we have the available, dedicated staff to meet the County's needs.

TEAM COMMITMENT

KCA and our proposed subconsultants have committed to provide the staff identified in this proposal. This commitment is based on having provided professional services to Pinellas County for more than 35 years including design of roadways, bridges, drainage systems, and environmental permitting services. Our experienced team will continue to provide the County with a high-quality product that is within budget and on schedule. The key staff identified on our organizational chart are immediately available to work. We have studied our current and future workload and determined that many of our current projects are nearing completion, freeing up staff to work on this contract.

We have reviewed the staffing availability over the next 24 months for the key team members and determined that they will all have adequate time to complete all assigned tasks. **Our PM, Ms. Lawson, is 100% committed to this project.** The chart below shows our availability for this project. Due to our large staff including multiple disciplines at KCA, we have a surplus of staff capable of working on this project if unforeseen conditions occur to keep this project on schedule and within the County's budget.



■ Staff utilization (workload percentages in blue) | ■ Available staff resources (availability percentages in gray)

05. Required Forms

 \mathbf{A}^{s}

s requested in Pinellas County's RFP, KCA has included the following required forms in this section:

- Acknowledgment of Addenda
- W-9
- Vendor References (4)
- Contract acceptance form including references and electronic payment
- E-Verify Affidavit
- SBE Status Form Refer to Tab 2, Page 6

ADDENDUM ACKNOWLEDGMENT

KCA acknowledges receipt of the following addenda on the specified date:

- Addendum 1 February 7, 2025
- Addendum 2 February 20, 2025

Form W-9
(Rev. March 2024)
Department of the Treasury
Internal Revenue Service

Request for Taxpayer Identification Number and Certification

Go to www.irs.gov/FormW9 for instructions and the latest information.

Give form to the requester. Do not send to the IRS.

Betor	Name of entity/individual. An entry is required. (For a sole proprietor or disreentity's name on line 2.)		wner's name on line	1, and enter the business/disregarded			
	Kisinger Campo & Associates Corp.						
	2 Business name/disregarded entity name, if different from above.						
ype. tions on page 3.	3a Check the appropriate box for federal tax classification of the entity/individuonly one of the following seven boxes. ☐ Individual/sole proprietor ☐ C corporation ☑ S corporation ☐ LLC. Enter the tax classification (C = C corporation, S = S corporation, F Note: Check the "LLC" box above and, in the entry space, enter the appropriation of the LLC, unless it is a disregarded entity. A disregarded.	Partnership P = Partnership) P = Partnership) P = Partnership	Trust/estate	4 Exemptions (codes apply only to certain entities, not individuals; see instructions on page 3): Exempt payee code (if any) Exemption from Foreign Account Tax			
Print or type. c Instructions	classification of the LEC, unless it is a disregarded entity. A disregarded box for the tax classification of its owner. Other (see instructions)	Compliance Act (FATCA) reporting code (if any)					
Specifi	3b If on line 3a you checked "Partnership" or "Trust/estate," or checked "LLC" and you are providing this form to a partnership, trust, or estate in which this box if you have any foreign partners, owners, or beneficiaries. See instru	you have an ownership	interest, check	(Applies to accounts maintained outside the United States.)			
See	5 Address (number, street, and apt. or suite no.). See instructions.		Requester's name	and address (optional)			
0)	One Tampa City Center, 201. N. Franklin St., Suite 900						
	6 City, state, and ZIP code						
	Tampa, Florida 33602						
	7 List account number(s) here (optional)						
Par	Taxpayer Identification Number (TIN)						
Pa	your TIN in the appropriate box. The TIN provided must match the name	o given en line 1 to av	social se	ecurity number			
backi reside	your TIN In the appropriate box. The TIN provided must match the harm up withholding. For individuals, this is generally your social security num ent alien, sole proprietor, or disregarded entity, see the instructions for F ss, it is your employer identification number (EIN). If you do not have a n	ıber (SSN). However, f Part I, later. For other	for a				
TIN, I		ambor, see now to ge	or Employer identification number				
Note	If the account is in more than one name, see the instructions for line 1. see To Give the Requester for guidelines on whose number to enter.	See also What Name		- 1 6 7 7 1 4 5			
Par	t II Certification						
	r penalties of perjury, I certify that:						
1. The 2. I at Se no	e number shown on this form is my correct taxpayer identification numb m not subject to backup withholding because (a) I am exempt from back rvice (IRS) that I am subject to backup withholding as a result of a failure longer subject to backup withholding; and	kup withholding, or (b)	I have not been r	notified by the Internal Revenue			
	m a U.S. citizen or other U.S. person (defined below); and						
4. Th	e FATCA code(s) entered on this form (if any) indicating that I am exemp	of from FATCA reportif	ng is correct.	whicet to back up withholding			
becau	fication instructions. You must cross out item 2 above if you have been n use you have failed to report all interest and dividends on your tax return. F sition or abandonment of secured property, cancellation of debt, contribut than interest and dividends, you are not required to sign the certification, t	or real estate transactions to an individual re	ons, item 2 does n tirement arrangem	ot apply. For mortgage interest paid, ent (IRA), and, generally, payments			
Sigr Her		1	Date 1/2	1/2025			
Ga	neral Instructions	New line 3b has b	een added to this	s form. A flow-through entity is			
Secti	on references are to the Internal Revenue Code unless otherwise d.	foreign partners, ow to another flow-thro	required to complete this line to indicate that it has direct or indirect foreign partners, owners, or beneficiaries when it provides the Form W-9 to another flow-through entity in which it has an ownership interest. This				
relate	re developments. For the latest information about developments and to Form W-9 and its instructions, such as legislation enacted they were published, go to www.irs.gov/FormW9 .	change is intended to provide a flow-through entity with information regarding the status of its indirect foreign partners, owners, or beneficiaries, so that it can satisfy any applicable reporting requirements. For example, a partnership that has any indirect foreign					
	at's New	partners may be red	quired to complete	e Schedules K-2 and K-3. See the es K-2 and K-3 (Form 1065).			
this li	3a has been modified to clarify how a disregarded entity completes ne. An LLC that is a disregarded entity should check the	Purpose of F		en en en emple film frank film en en film film film film film film film film			
appropriate box for the tax classification of its owner. Otherwise, it should check the "LLC" box and enter its appropriate tax classification.		An individual or entity (Form W-9 requester) who is required to file an information return with the IRS is giving you this form because they					

Cat. No. 10231X Form **W-9** (Rev. 3-2024)

CONTRACTOR REFERENCES

Company Hames Kisinger Campo & Associates, Corp. (KCA)

Business Address: 201 N. Franklin Street, Suite 900, Tampa, FL 33602

Length of time the company has been in business: 48 years

How long in present locations 4 months

Total number of current employees: Full-Time: 350 Part-Time: 326

Number of employees you plue to use to service this contract: 45 (including subconsultants)

All references will be contacted by a County Designee via email, fax, or phone call to obtain answers to questions, as applicable before an evaluation decision is made. Vendor must have experience in work of the same or similar nature, and must provide references that will satisfy the County. Proposer must furnish a reference list of at least four (4) customers for whom they have performed similar services.

REFERENCE 1:	REFERENCE 2:
Monroe County	Сэпрэлу: City of Clearwater
Address: 1100 Simonton Street Key West, FL 33040	100 S. Myrtle Ave. Clearwater, FL 33756
Telephane: 305.295.4306	Telephone: 727.562.4760
Clark Briggs	Contact Name: Michael Quillen, PE
Contact Email: briggs-clark@monroecounty-fl.gov	Email: michael.quillen@myclearwater.com
Company Email: N/A	Company Email: N/A
REFERENCE 3:	REFERENCE 4:
City of St. Petersburg	Company: Pasco County
One 4th St N, MSC, St. Petersburg, FL 33701	Address 5418 Sunset Road New Port Richey, FL 34652
Telephone: 727.551.3499	Telephone: 727.834.3604 ext. 1610
Contact Name: Evan Birk, PE	Contact Name: Whitney Haddox, PE, MCE
Contact Email: Evan.Birk@stpete.org	Contact Email: whaddox@mypasco.net
Company Email: N/A	Company Email: N/A

THIS FORM MUST BE RETURNED WITH YOUR CHECKLIST



KCA's SunBiz

2025 FLORIDA PROFIT CORPORATION ANNUAL REPORT

DOCUMENT# 505419

Entity Name: KISINGER CAMPO & ASSOCIATES, CORP.

FILED
Jan 24, 2025
Secretary of State
6358818513CC

Current Principal Place of Business:

ONE TAMPA CITY CENTER 201 N FRANKLIN STREET STE 900 TAMPA. FL 33602

Current Mailing Address:

ONE TAMPA CITY CENTER 201 N FRANKLIN STREET STE 900 TAMPA, FL 33602 US

FEI Number: 59-1677145 Certificate of Status Desired: Yes

Name and Address of Current Registered Agent:

SUTTON, KEVIN HILL WARD HENDERSON 101 E. KENNEDY BLVD SUITE 3700 TAMPA, FL 33606 US

The above named entity submits this statement for the purpose of changing its registered office or registered agent, or both, in the State of Florida.

SIGNATURE: KEVIN SUTTON 01/24/2025

Electronic Signature of Registered Agent Date

Officer/Director Detail:

Title OFFICER, CHAIRMAN, SR VP Title OFFICER, PRESIDENT, CEO

Name CAMPO, MICHAEL J Name FOLEY, PAUL G.

Address ONE TAMPA CITY CENTER Address ONE TAMPA CITY CENTER

201 N FRANKLIN STREET STE 900 201 N. FRANKLIN STREET STE 900

City-State-Zip: TAMPA FL 33602 City-State-Zip: TAMPA FL 33602

Title OFFICER, SR. VP, SECRETARY, Title OFFICER, SR VP
TREASURER, CFO

Name GOTT, RONALD E

Address ONE TAMPA CITY CENTER

Address ONE TAMPA CITY CENTER ONE TAMPA CITY CENTER 201 N FRANKLIN STREET STE 900

201 N. FRANKLIN STREET STE 900

City-State-Zip: TAMPA FL 33602

Title OFFICER, SR VP

Name MCGUCKEN, STEPHEN
Name MARTIN. GEORGE DEWEY

Address ONE TAMPA CITY CENTER ONE TAMPA CITY CENTER 201 N FRANKLIN STREET STE 900

201 N FRANKLIN STREET STE 900 City-State-Zip: TAMPA FL 33602

Title OFFICER, VP

Title OFFICER, VP Name O'GRADY, PATRICK

Name HARRISON, RICHARD

Address ONE TAMPA CITY CENTER

ONE TAMPA CITY CENTER 201 N FRANKLIN STREET STE 900 201 N FRANKLIN STREET STE 900

City-State-Zip: TAMPA FL 33602

Continues on page 2

I hereby certify that the information indicated on this report or supplemental report is true and accurate and that my electronic signature shall have the same legal effect as if made under oath; that I am an officer or director of the corporation or the receiver or trustee empowered to execute this report as required by Chapter 607, Florida Statutes; and that my name appears above, or on an attachment with all other like empowered.

SIGNATURE: RONALD GOTT CFO 01/24/2025

Electronic Signature of Signing Officer/Director Detail



Address

KCA's SunBiz

Officer/Director Detail Continued:

201 N FRANKLIN STREET STE 900

Title OFFICER, VP Title OFFICER, VP

Name MADRIZ, GUILLERMO Name CULLUM, SAMUEL

Address ONE TAMPA CITY CENTER Address ONE TAMPA CITY CENTER

201 N FRANKLIN STREET STE 900

City-State-Zip: TAMPA FL 33602 City-State-Zip: TAMPA FL 33602

Title OFFICER, SR VP Title OFFICER, VP
Name HILTON, GERALD Name FOLEY, SEAN

Address ONE TAMPA CITY CENTER Address ONE TAMPA CITY CENTER

201 N FRANKLIN STREET STE 900 201 N FRANKLIN STREET STE 900

City-State-Zip: TAMPA FL 33602 City-State-Zip: TAMPA FL 33602

Title OFFICER, VP Title OFFICER, VP

Name DIGGS, DAVIS Name BARBOUR, DEBORAH

Address ONE TAMPA CITY CENTER Address ONE TAMPA CITY CENTER

201 N FRANKLIN STREET STE 900 201 N FRANKLIN STREET STE 900

City-State-Zip: TAMPA FL 33602 City-State-Zip: TAMPA FL 33602

Title OFFICER, VP Title OFFICER, VP

Name LOPER, KELLIE Name ABDALLA, MOHAMED

Address ONE TAMPA CITY CENTER Address ONE TAMPA CITY CENTER

201 N FRANKLIN STREET STE 900 201 N FRANKLIN STREET STE 900

City-State-Zip: TAMPA FL 33602 City-State-Zip: TAMPA FL 33602

Title OFFICER, VP Title OFFICER, VP

Name ROBERTS, CHRISTOPHER Name COMBS, RICHARD

Address ONE TAMPA CITY CENTER Address ONE TAMPA CITY CENTER

201 N FRANKLIN STREET STE 900 201 N FRANKLIN STREET STE 900

City-State-Zip: TAMPA FL 33602 City-State-Zip: TAMPA FL 33602

ePayables Registration

The Pinellas County ePayables credit card payment program is a convenient payment option provided in connection with Bank of America. The County will remit payments through an issued ghost (non-physical plastic) Visa credit card number. Funds are typically deposited within 48 hours.

To participate in the program, your company must be able to accept credit card payments through merchant services. When payment is authorized, you will receive an automated email notification with invoice number(s), invoice date(s), and payment amount(s). To deposit funds into your account, input the Visa card number on your credit card point of sale (POS) terminal for the full payment amount authorized. The card will return to a zero balance until the next payment is issued.

sale (POS) terminal for the full payment amount authorized. The card will return to a zero balance until the next payment is issued.
Upon registration a secure welcome email will be sent with a 15-digit account number and expiration date. Payment confirmation will be remitted via email.
Would your company like to enroll in the ePayables credit card payment program? Yes Mo 🗶
Please select your perferred email notification option:
Default email notification: vendor leeps 16-digit account number on-file A 16-digit account number will be issued upon initial ePayables enrollment. Once a payment has been applied, an
email notification is sent with the last 4-digits as payment confirmation. The County will provide the 3-digit CVV code if needed.
 Secure email notification: vendor does not keep 16-digit account number on-file Once a payment has been applied, a encrypted secure email notification is sent with the full 16-digit account number and expiration date as payment confirmation. The County will provide the 3-digit CVV code if needed.
3) Does your company require a 3-digit CVV for credit card processing? Yes No
ePayables Supplier Enrollment Information
Company merchant names
Remittance address:
Catys
State
Zip code:
ePayables contact names
Title:
Phone:
Remittance email(s):
*AR/Receivables Department sensit or designated person to process card transaction payments
*Standard marchine flux upply, and may be regativable through year marchine (f applicable). Finallies County does not charge to participate in the program, and credit cord transaction flux cannot in passed on to the County.
Please contact Natalie Ryburn, Purchasing Systems Coordinator, for questions or further assistance.

Email: pinellascountyepayablesprogram@pinellas.gov

Phone: 727-453-3011

E-VERIFY AFFIDAVIT

E-VERIFY AFFIDAVII
I hereby certify that <u>Kisinger Campo & Associates, Corp.</u> [insert contractor company name] does not employ, contract with, or subcontract with an unauthorized alien, and is otherwise in full compliance with Section 448.095, Florida Statutes.
All employees hired on or after January 1, 2021 have had their work authorization status verified through the E-Verify system.
A true and correct copy ofKisinger Campo & Associates, Corp[insert contractor company name] proof of registration in the E-Verify system is attached to this Affidavit. Signature: Print Name Guillermo Madriz, PE, Vice President Date: February 27, 2025
Federal Work Authorization User Identification No.: 163673
Name of Pinellas County Contract and Contract No.: McMullen Booth Rd Bridge North STATE OF FLORIDA COUNTY OF Hillsborough The foregoing instrument was acknowledged before me by means of 1) physical presence _vor 2) online notarization, thisEebruary 27, 2025 (date) by Guillermo Madriz, PE (name of officer or agent, title of officer or agent) of Kisinger Campo & Associates, Corp (name of contractor company acknowledging), a
(state or place of incorporation) corporation, on behalf of the corporation. He/she is personally known to me or has produced(type of identification) as identification.
[Notary Seal]
Notary Public: Langue
Name typed, printed, or stamped: Theresa Sansone
My Commission Expires:



An official website of the United States government Here's how you know



Menu ≡

My Company Account

My Company Profile

Company Information

Company Name

Kisinger Campo & Associates Corp.

Doing Business As (DBA) Name

Company ID

163673

Enrollment Date

Nov 12, 2008

Employer Identification Number (EIN)

591677145

Unique Entity Identifier (UEI)

DUNS Number

Total Number of Employees

100 to 499

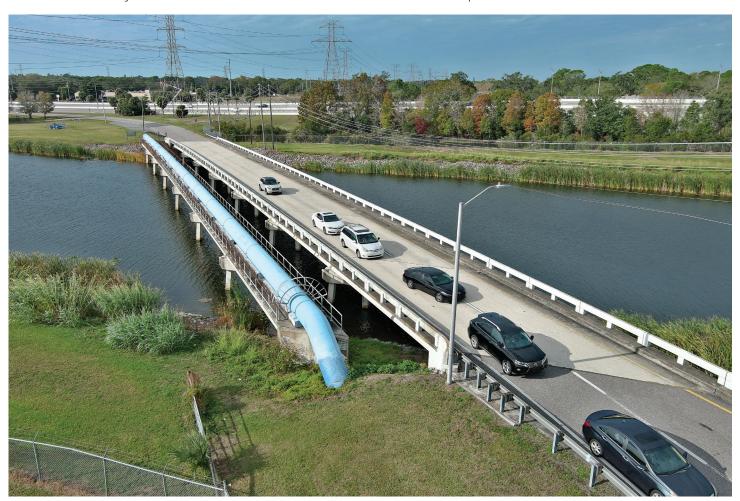


06. Additional Information

INTRODUCTION TO PROJECT UNDERSTANDING AND HISTORY

he McMullen Booth Road (CR 611) northbound off-ramp at Tampa Road is the location of Bridge No. 154151 over the Lake Tarpon Outfall Canal. County Road 611, commonly known as McMullen Booth Road (south of Tampa Road) and East Lake Road (north of Tampa Road), is a major north-south arterial connecting south Pinellas County to Pasco County. The NB Tampa Road exit provides access to the unincorporated East Lake area. The off-ramp bridge, constructed in 1967, is approaching the end of its design life and has been rated functionally obsolete according to the latest bridge inspection report (July 11, 2024). An adjacent bridge structure supporting a 48" Pinellas County water main is located approximately ten feet parallel to the off-ramp bridge. Although the water main is not part of this scope of work, it will be a consideration and point of coordination during the design of the off-ramp bridge replacement.

The most recent routine bridge inspection report documented intermittent spalls and delaminations in the deck slabs and overhangs with locations of exposed deck rebar. The inspection report notes areas of exposed rebar in nearly all the precast deck panels due to insufficient concrete cover. It was also observed the majority of the existing 18" prestressed piles exhibit corner spalling along with areas of delaminations. The existing bridge was found to have a sufficiency Rating of 63.7 and a Health Index of 73.83. The National Bridge Index (NBI) ratings were 6 Satisfactory for the Deck and Substructure and 7 Good for the Superstructure.



McMullen Booth Road Bridge North Bound (existing)

Previous Studies

As part of project development, the County and FDOT have completed multiple reports and studies including the Phase I – Bridge Replacement Study (2012) and the Phase II - Bridge Replacement Report (2017) both prepared by KCA recommending bridge replacement. FDOT reports include regular Bridge Inspection Reports along with Phase 1 and 2 Scour Evaluation Reports.

This bridge along with both the northbound and southbound mainline bridges (also programmed for replacement as a separate design project) are part of the broader East Lake Road corridor study area. The County has developed an Existing Conditions Assessment (2021) and Preliminary Corridor Analysis and Alternatives Development Study Report (2022) and is now preparing to conduct a Project Development and Environment (PD&E) Study. Once underway, the selected design team for the off-ramp bridge replacement will need to coordinate closely with the design teams for the mainline bridge replacements and the PD&E team.

KCA has extensive knowledge of the East Lake Road Corridor and Pinellas County processes with involvement in the following projects in addition to the above mentioned bridge reports:

- PID 000967A Pinellas Trail Loop North Segment Design Build
 - KCA Design Team
 - Construction complete



Pinellas Trail Loop North Segment

- PID 000967C Pinellas Trail North Gap Tampa Rd to E Lake Rd S Bridge over Lake Tarpon Outfall Canal (LTOC)
 - KCA Design Team
 - Construction nearing completion



Bridge Over LTOC in construction

- PID 003879B East Lake Road Bridges over Brooker Creek
 - KCA is the selected design team
 - Awaiting Board Approval



Bridges over Brooker Creek (existing)

Scope – Understanding

KCA understands the primary goal of this scope of services is to design the replacement of the off-ramp bridge over the Lake Tarpon Outfall Canal. Final plans and specifications will be produced in accordance with Pinellas County's Civil 3D kit and meet the County's standards according to County and FDOT design criteria and specifications. Although the bridge replacement report recommended a three-lane typical section with shoulders and sidewalks on both sides, we believe other alternatives should be analyzed to meet the traffic needs and County's long-term objectives for multi-modal connectivity. Using the completed reports as a basis, KCA will evaluate typical sections and alignment alternatives to develop a Draft Alternatives Analysis Report. With input from the County, the preferred alternative will be selected and included in the Final Alternatives Analysis Report to initiate design.

Potential Issues and Key Design Elements

Existing R/W (IAA)

The existing bridge and adjacent 48-inch water main structure are within approximately 120 feet of Pinellas County R/W over the Canal. According to the County's Property Appraiser website, the adjacent land is owned by Southwest Florida Management District (SWFWMD) and Boot Ranch Partnership. The Alternatives Analysis Report will include R/W impacts for each alternative.

IAA will provide the necessary expertise and staff needed to conduct a Preliminary Design and Engineering (PD&E) level R/W Cost Estimate. Services shall include conducting estimates of market value of all real estate interests to be acquired as part of the preliminary design and alternative analysis and preparing a written R/W cost estimate/report. IAA will be required to evaluate, quantify and report on the alignment alternatives for the subject project as required. The IAA team will research recent comparable sales, current listings and/or valuation data necessary to support an estimate of R/W land, severance damages and/or cost to cure costs and will further take into consideration, estimate, and report all other R/W acquisition costs by general and customary factors, multipliers, historical data, and generally accepted practices.

Existing Utilities: DUKE transmission and distribution, and Pinellas County Water Main

An existing 48-inch steel water main runs parallel to the off-ramp bridge approximately ten feet away on the east side. It is supported by a separate bridge structure which was likely built at the same time as the off-ramp bridge. Coordination with Pinellas County Utilities Department will be critical in determining the off-ramp alignment with consideration of the water main disposition.

Duke Energy is also a major utility owner having both transmission and distribution lines within the project footprint. 13 kV distribution lines run parallel to the bridge approximately 27-feet to the west and 500 kV transmission lines cross the approach roadway just south of the bridge. Off-ramp alignment alternatives will avoid the transmission lines and towers and the bridge abutment pile driving operations will need to take the transmission lines into consideration during construction.

In additional to these overhead utilities, SUE survey will be required to determine the location and impact of buried utilities during design.



Utility Coordination Support

KCA will provide support to the County's utility coordinator throughout the design process. Utility owner RGBs lines will be displayed on the utility adjustment sheets in conjunction with the survey and SUE data. Potential utility conflicts will be analyzed and tracked during design using a utility conflict matrix. KCA will be in attendance of all utility coordination meetings using the matrix as a guide to resolve or determine conflict impacts for each utility owner. Prior to final plans, KCA will review and sign-off on all Utility Work Schedules (UWS) for inclusion in bid documents.

SUE (C&F)

C&F's SUE approach will be to provide Quality Level "B" Designation early in the project schedule for all potential conflict areas. Additionally, Quality Level "A" SUE will be utilized to clear the proposed below-ground improvements, such as light poles, sign structures, signals, drainage structures, or any other new underground infrastructure and earthwork excavation. C&F Survey crews will locate all SUE Level "B" Designation and Level "A" Test Holes and incorporate the data into the Design Survey file, as well as providing a SUE Test Hole Report. C&F has the recent SUE experience with Pinellas County, and the availability to have multiple crews and office personnel working jointly to meet the schedule for this project.

Roadway

McMullen Booth Road is a 6-lane divided Principal Arterial roadway with a C3R context classification. The existing single-lane exit ramp to Tampa Road features a 15-foot travel lane with 6-foot inside and 4-foot outside shoulders before opening to two 11-foot travel lanes with 3-foot inside and outside shoulders crossing Lake Tarpon Outfall Canal (Bridge No. 154151). The typical section widens after the bridge to four lanes with two 11-foot and two 12-foot travel lanes and curb and gutter on each side as the ramp approaches Tampa Road (CR 752).

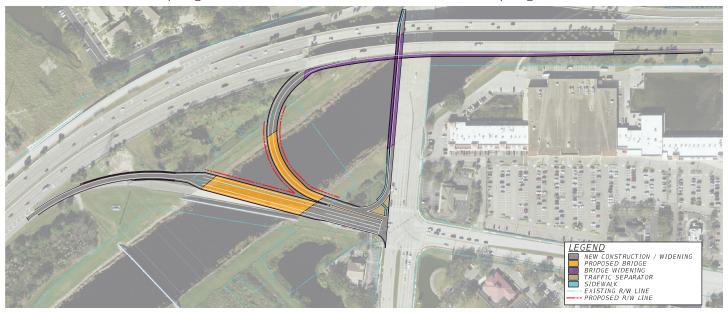
The intent of the project is to reconstruct Bridge No. 154151 as it is rated as functionally obsolete. To accomplish this, the design team is focusing on constraints within the project limits. These include, but are not limited to, the transmission tower between the ramp and McMullen Booth Road mainline, the above-ground water main running parallel to Bridge No. 154151, and maintaining traffic in the temporary conditions to ensure traffic does not back up to McMullen Booth Road.

By establishing the main constraints of the corridor, the proposed concept can begin to form. The design team is keen to maintain a single-lane exit ramp from McMullen Booth Road that widens to provide four 11-foot travel lanes across Lake Tarpon Outfall Canal into Tampa Road. This typical section will include

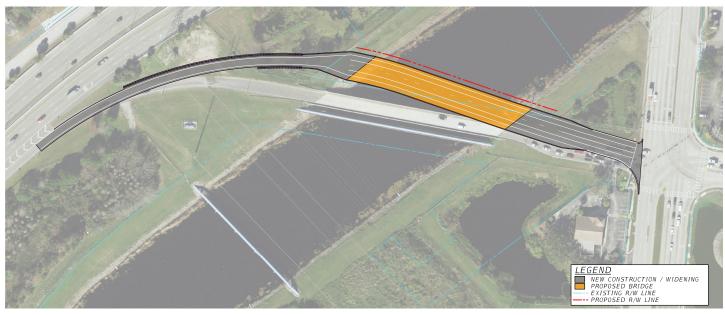
6-foot inside and 10-foot outside shoulders across the canal before transitioning the shoulders in to provide curb and gutter tie-ins to the existing Tampa Road intersection. Guardrail can be used along the single-lane typical section to shield the transmission tower and ensure no adverse impacts or relocation costs. The alignment of the proposed roadway can avoid impacts to the above-ground water main and allow for two lanes of traffic to be maintained while the proposed bridge is being phase-constructed.

KCA will perform an Alternatives Analysis which will investigate typical section, horizontal alignment, R/W impaces, and bridge alternatives. We have started this investigation with the following three alignment alternatives.

Alternative 1: NB off-ramp alignment which also accommodates a NB on-ramp alignment.



Alternative 2: NB off-ramp alignment maintain traffic on the existing bridge with phased construction of the proposed bridge.



LEGEND WE CONSTRUCTION / WIDENING PROPOSED BRIDGE PROPOSED BRI

Alternative 3: NB off-ramp and bridge in the existing footprint and R/W.

Horizontal Alignment Alternatives

The proposed alignment will be dependent on various constraints throughout the corridor as well as the desired elements of the proposed bridge. The constant between the three alternatives is the need for a superelevated curve after departing the McMullen Booth Road mainline.

The proposed northbound off-ramp alignment in **Alternative 1** utilizes a superelevated curve from the existing ramp terminal that ties into a tangent segment that spans Lake Tarpon Canal and into the Tampa Road intersection. The northbound on-ramp in **Alternative 1** utilizes a minimum radius curve to tie southbound Tampa Road to McMullen Booth Road. The use of a minimum radius curve will create the need for a 14-foot inside shoulder to accommodate stopping sight distance requirements for 30 mph.

Alternative 2 focuses on maximizing the work zone of the proposed structure across Lake Tarpon Canal. To achieve this, a horizontal deflection was utilized from the existing Tampa Road intersection. A normal crown curve was then placed along the deflection line, creating ample space for phase construction of the proposed structure while keeping the existing structure intact during the first phase of construction. Two lanes of traffic can then be moved to the proposed structure while demolition of the existing structure occurs before widening the proposed structure to its final condition.

Alternative 3 matches the bearing of the existing R/W line across Lake Tarpon Canal. This ensures no R/W acquisition is needed to construct the new bridge. Like Alternative 2, it utilizes a normal crown curve and horizontal deflection to tie into the existing Tampa Road intersection.

Design Variations/Exceptions

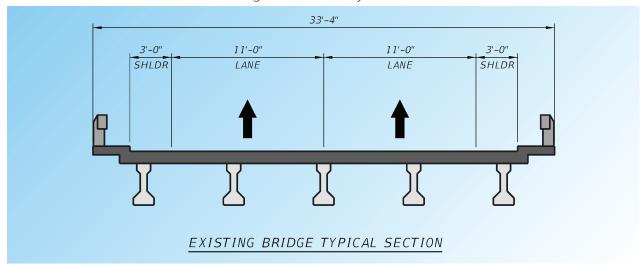
Alternative 3 utilizes the existing R/W corridor to eliminate the need for R/W acquisition. This alternative aligns the inside edge of the proposed bridge as close to the existing R/W line as possible, which will create the need for a border width variation.

All alternatives require a superelevated curve from the McMullen Booth mainline to the tangent section crossing Lake Tarpon Canal. In order to keep superelevation transition off of the proposed bridge, a design variation for superelevation rate may be necessary.

Due to the short distance between the end of the proposed bridge and the existing intersection with Tampa Road, a horizontal curve with a length less than 400 feet may be needed to provide enough deflection to accommodate the traffic needs during the temporary traffic control phases.

Structures

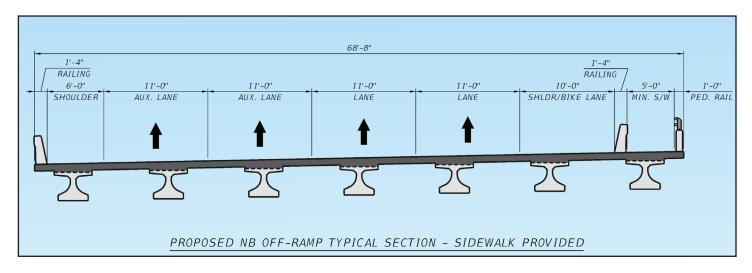
The CR 611 (McMullen Booth Road) northbound off-ramp over Lake Tarpon Outfall Canal (Bridge No. 154151) provides access to CR 752 (Tampa Road) and East Lake Road Business in Pinellas County Unincorporated area of Palm Harbor. The existing bridge, supported by prestressed AASHTO Type II beams, consists of seven equal spans of 56'-8" totaling a length of 396'-8". The current width of the bridge is 33'-4", with a curb-to-curb width of 28'-0". This horizontal clearance classifies the bridge as functionally obsolete due to the substandard shoulder widths.



Proposed Bridge Typical Section

The proposed typical section of the bridge would provide lane and shoulder widths compliant with current design codes. Preliminary traffic count numbers indicate that the vehicular queue length for the northbound left turn lane onto Tampa Road would cause peak hour backups overflowing onto the existing bridge. To accommodate anticipated future queue lengths for the dual left turn lanes, two additional lanes are proposed to allow through and right turn movements which would otherwise be blocked by the left turn queue. The proposed typical section will provide two (2) 11'-0" auxiliary lanes, two (2) 11'-0" travel lanes, a 6'-0" inside shoulder and a 10'-0" outside shoulder.

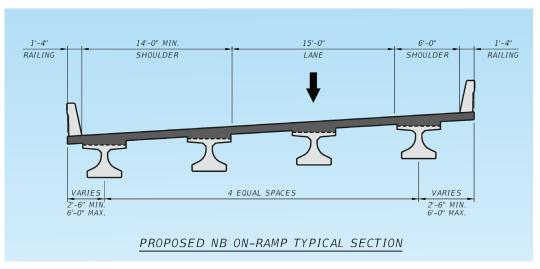
Coordination with the county will be conducted to accommodate any pedestrian and bicycle facilities on the bridge. The proposed typical section can accommodate a bike lane within the 10'-0" outside shoulder as well as a protected sidewalk on the east side of the bridge. This would allow for continuity across the bridge if pedestrian access through the Northbound McMullen Booth corridor is provided in the future and would preclude the need for potential widenings. Accounting for pedestrian facilities now can ensure the efficient development of a future multimodal transportation system through this area.



Superstructure Considerations

The bridge site is located upstream of a SWFWMD water control structure (S-551), essentially severing navigability for marine interests from Old Tampa Bay. However, small boats and personal watercrafts were observed using the Lake Tarpon Outfall Canal in the vicinity of the bridge. The intent of the proposed bridge would be to maintain—or improve—the navigation envelope (horizontal and vertical clearances) of the bridge by utilizing a similar superstructure height. This would involve considering alternatives that utilize either AASHTO Type II beams, which match the current superstructure type, or Florida-I 36 Beams (FIB 36), which maintain the same superstructure depth. FIB 36 beams provide more load carrying capacity than AASHTO Type II beams, increasing maximum span lengths and reducing the number of required spans to achieve the same bridge length. This would reduce the total substructure units required and increase the horizontal clearance of the channel. However, during the design phase each alternative will be evaluated to determine the most cost-effective superstructure option.

For the proposed northbound CR 611 on-ramp bridge in Alternative 1, the superstructure is constrained due to the tight curve radius of the alignment. This curvature would result in a varying overhang width along the length of the span when straight prestressed concrete beams are utilized. Due to the wider top flange widths of FIB beams, as well as increased capacity, they are able to support a wider overhang than AASHTO Type II beams. For this reason, they are the proposed superstructure type when considering this alternative.



Substructure and Foundation Considerations

The existing bridge is supported on pile bents founded on 18-inch prestressed concrete piles. The proposed bridge replacement would also utilize pile bents as the preferred alternative. Based on the final loading, geotechnical exploration and environmental classification of the area, either 18-inch or 24-inch prestressed concrete piles would likely be used as the foundation for the pile bents. Our team's experience with the Pinellas Trail bridge just north of the project location suggests the subsurface conditions at the bridge site are likely to consist of sandy to clayey soils, underlain by very hard indurated clay and weathered limestone. Standard Penetration Test (SPT) borings will be required during the design phase of the project to assist in determining the most cost-effective foundation type and to estimate foundation depths. The Pinellas Trail Bridge, currently under construction, is founded on 24-inch concrete piles and required preforming operations to reach minimum tip due to the hard soil layers above the required minimum tip elevation.

Multi-modal Accommodations

As referenced in the above sections, the proposed bridge can be designed to accommodate pedestrians and cyclists. These provisions would allow connectivity to the Pinellas Trail on the west side of McMullen Booth Road or the Oldsmar Trail on the east side of Lake Tarpon Canal should either trail be extended to the project limits in the future.

There is also an existing sidewalk along the north side of Curlew Road that truncates at its intersection with McMullen Booth Road. If a sidewalk is constructed along the east side of McMullen Booth Road, connectivity from Curlew Road to Tampa Road could be accomplished without any bridge widening.

Drainage

The McMullen Booth Bridge (No. 154151) over Lake Tarpon canal is located in FEMA Flood Zone AE with an elevation of 6 feet. KCA will develop a Bridge Hydraulic Report (BHR) including a hydraulic analysis of the existing and proposed bridge along with scour

calculations, taking into account sea level rise and resiliency. In addition, we will examine the surrounding area for drainage impacts. There are existing Pinellas County stormwater ponds located northwest (Detention Pond 14), northeast (Detention Pond 15B) and southeast (Detention Pond 15A) of the bridge. Due to their proximity, the proposed bridge and roadway will impact part of the existing ponds. Compensation for these impacts will be reviewed including reshaping the ponds within the available R/W, reducing the berm width and providing compensation with an adjacent pond. We will field inspect the work site and assess all existing drainage structures and drainage patterns. The existing bridge utilizes scuppers to drain roadway runoff off the bridge. The proposed bridge will be evaluated for spread to determine the need for scuppers or if the runoff can be collected at the ends of the bridge. Roadway runoff will be collected with inlets and a storm sewer system. The project discharges to Lake Tarpon Canal (WBID 1541B) which is not impaired or an OFW. If the project includes additional travel lanes, stormwater treatment and attenuation will be required. Based on the new pollutant loading criteria, the project would need to reduce nitrogen by 55% and phosphorous by 80%. This could be accomplished by constructing a shallow bioswale southwest of the bridge within existing R/W to treat the proposed bridge using compensatory treatment. The bioswale would need to be configured to avoid the existing mitigation side to the south as well as the existing water main. Temporary drainage will be analyzed for the project to ensure positive drainage during construction.

Traffic

The intersection at Tampa Road and East Lake Road is nearly at capacity in the current year and is expected to fail in the design year. Lane additions and intersection reconstruction were not the most feasible alternatives given the current configuration and proximity to nearby businesses. Our team found the proposed eastbound right-turn lane along Tampa Road provides the needed capacity at the intersection. Along with signal phasing modifications, this improvement is expected to operate at an acceptable LOS in the design year. The added right-turn movement redistributes the eastbound left-turn volume and allows additional green time for the westbound through movement. Right-turn phasing overlap with their respective left-turns is recommended since 3 of the 4 movements carry significant traffic.

Signal

Our Team has extensive experience with signalization reconstruction. The proposed mast arm locations will meet the required horizontal clearance from the edge of travel way. We will coordinate with the PD&E study to ensure the locations do not conflict with any proposed future improvements. A challenge our team

has recently experienced is designing new mast arms along approaches with nearby utility poles/wires. Our team is familiar with the OSHA clearance requirements and coordination needed to properly install signal poles near utility poles/wires.

Our design will include retroreflective yellow borders on all signal heads and LED overhead street name signs on each signal to improve safety and reduce rear-end crashes. The intersection will require three 5-section right-turn signal heads to accommodate the desired overlap between the left-turn and right-turn movements. We will utilize our KCA invented signal timing program to provide adequate signal clearance timings for all movements. All pedestrian features will be provided to help improve safety and meet ADA compliance. All yellow/all-red vehicular clearance timings will be updated. We will provide "Turning Vehicles Yield to Pedestrians" (R10-15a) Signs to improve pedestrian and bicyclist safety.



Proposed Bioswale for Pollutant Loading Reduction

S&PM

Signing and Pavement Markings design will provide positive guidance to motorists and will meet all current criteria and standards. Our team will take inventory of all existing signs within the project limits. All signs over five years old or having inadequate retroreflectivity levels will be replaced or upgraded to current standards. We will optimize sign locations for visibility to provide clear messages that facilitate drivers' decision making. Our team will provide the necessary wrong-way signage (R5-1 and R5-1a signs) and arrows. The advanced street name guide sign will be replaced along the ramp approach.

Lighting

There is existing lighting at the intersection and along the ramp. Light poles impacted by the bridge reconstruction will be replaced to provide adequate illumination along the roadway and at the intersection. We will provide photometric analysis for the signalized intersection and pedestrian crossing separately to

optimize the location of the light fixtures to ensure adequate and safe illumination for all users. We will ensure achieving the required horizontal and vertical illumination for the intersection and crosswalks. Our team understands the importance of the coordination with Pinellas County required on this project to ensure adequate maintenance of any new lighting is approved.

Environmental/Permitting Considerations

Protected species involvement is expected to be minimal. The project is within the core foraging area of one active colony of the federally listed wood stork, but we do not anticipate any loss of available foraging habitat. Manatees are known to transit the lower reach of the Canal and adherence to the Manatee Construction Conditions during construction will be required as a permit condition. No federal designated critical habitat or essential fish habitat is present within or adjacent to the project area. Presently, the tricolored bat is proposed for listing under the Endangered Species Act; however, this species is not listed as potentially occurring in the project area by the U.S. Fish and Wildlife Service (USFWS) Information for Planning and Consultation (IPaC) online resource. If the tricolored bat is listed during project design, further coordination with USFWS will be required to determine if an acoustic survey is needed.

Minimal wetland habitat is present and consists chiefly of emergent aquatic vegetation along the banks of the Canal. Stone riprap lines the banks of the Canal above the waterline. Although wetland impacts will be minimal, the project does not qualify for the General Environmental Resource Permit (ERP) under 62-330.443 due to the addition of travel lanes. For this reason, it is expected that an Individual ERP will be required from the Southwest Florida Water Management District (SWFWMD). Shortly after NTP we will schedule a permit pre-application meeting with SWFWMD.

The Canal is a jurisdictional water of the U.S. and a Section 404/Section 10 permit from the U.S. Army Corps of Engineers (USACE) will be required. The project should qualify for USACE Nationwide Permit No. 14 (Linear Transportation Projects). USACE will also review the project's potential effect on the operation of the Lake Tarpon Outfall Canal under a Section 408 review. Coordination with the USACE for the Section 408 review should begin as part of the PD&E study. We will work closely with the County and the PD&E consultant to accurately reflect the PD&E Section 408 coordination in our USACE permit application.

In response to the East Lake Road PD&E study's Advance Notification within the FDOT Environmental Screening Tool, the U.S. Coast Guard (USCG) assigned a determination of effect of "Not Applicable/No Involvement" for navigation, thereby indicating a USCG

bridge permit is not required for any of the bridges over Lake Tarpon Outfall Canal.

Cultural Resources (Janus)

Janus Research has collaborated with the KCA on several bridge projects in Pinellas County, including the Venetian Boulevard Bridge, Snell Isle Bridge, and the Bayou Grande Bridge. Each of these projects necessitated the creation of documentation in compliance with State and/or Federal Regulations. The Bayou Grande Bridge, the subject of a replacement project, was determined eligible for inclusion in the National Register. Janus Research also collaborated with the KCA team to mitigate the adverse effects on the bridge, including the comprehensive Historic American Engineering Record (HAER) documentation.

Based on preliminary research, there are no previously recorded archaeological sites or historic parcels. However, FDOT Bridge No. 154151 (built 1967) is a historic bridge but may likely meet the Section 106 Program Comment due to its common bridge type. This project will necessitate the establishment of an Area of Potential Effect (APE), which is likely to encompass the bridge and the canal. There is a lithic scatter about 175 meters west on other side of McMullen Booth Road (Pl1247, not evaluated), but adjacent to the canal and it is likely disturbed with spoil. Within this APE, all historic and archaeological resources will be documented within a Cultural Resources Assessment (CRAS) report. These resources will then be evaluated in accordance with National Register criteria to determine their significance.

Contamination (Tierra/DPS)

Tierra understands that the purpose of this project is to design the replacement of the McMullen Booth Road (CR 611) Northbound Off-Ramp Bridge (Bridge No. 154151) over Lake Tarpon Outfall, along with any associated roadway improvements. To support this effort, Tierra proposes to conduct a contamination screening in accordance with the Florida Department of Transportation's (FDOT) PD&E Manual: Part 2. Chapter 20 (Contamination) and to perform an asbestos survey to ensure compliance with environmental regulations and protect public health. The contamination screening will identify potential hazardous materials, such as petroleum or heavy metals, that could impact construction activities and require proper mitigation. Additionally, the asbestos survey will assess the presence of asbestos-containing materials in the existing bridge structure, ensuring their safe handling and disposal in accordance with state and federal guidelines. These evaluations are essential for minimizing environmental risks, maintaining worker safety, and preventing costly project delays.

To identify potential contamination sites, Tierra reviewed relevant GIS databases maintained by the Florida Department of Environmental Protection (FDEP) for regulated facilities within 500 feet of the project corridor. This review identified one petroleum cleanup site with unresolved contamination conditions, presenting the highest potential for contamination involvement with the project. This site may require Level II testing:

■ Iberia Bank, 3100 Tampa Road, Oldsmar (former Mobil gas station).

Another key contamination consideration is the replacement of Bridge No. 154151 itself. Bridges can contain hazardous materials due to the presence of asbestos in construction materials or metals-based coatings. In accordance with FDOT's PD&E Manual, Chapter 20, an asbestos survey and screening for metals-based coatings should be conducted to determine the extent of these materials. If hazardous materials are confirmed, appropriate worker protection protocols and material handling procedures must be implemented during construction.

Geotechnical (Tierra/DPS)

Tierra's staff includes more than 30 geotechnical engineers. Tierra is prequalified for the following FDOT work groups required for the project: 9.1 Soil Exploration, 9.2 Geotechnical Classification Lab Testing, 9.3 Highway Materials Testing, 9.4 Foundation Studies, and 9.5 Geotechnical Specialty Lab Testing. Tierra's personnel have significant geotechnical experience in the project area, including the Pinellas Trail bridge over the canal just north of the subject project. Tierra's experience relative to this project is a valuable asset to both the design team and Pinellas County.

Tierra has reviewed the USDA NRCS Soil Survey for Pinellas County and the Pinellas Trail information to establish geotechnical considerations for this project. Provided below is a summary of the geotechnical considerations for your use.

Geotechnical Considerations - Roadway

Based on a review of the USDA NRCS Soil Survey of Pinellas County, pre-development normal seasonal high groundwater (SHGWT) levels along the project area are anticipated to range from about 1 foot to about 3½ feet below natural grades. However, based on our experience, groundwater levels are deeper than reported by the USDA as a result of the influence of the Lake Tarpon Outfall Canal. The surficial soils predominantly consist of sandy (A-3/A-2-4) soils.

Geotechnical Considerations – Bridge

Based on our experience with the Pinellas Trail bridge just north of the subject bridge, the subsurface conditions at the bridge site are anticipated to consist of sandy to clayey soils, underlain by very hard

indurated clay and weathered limestone. Standard Penetration Test (SPT) borings will be required during the design phase of the project to assist in determining the most cost-effective foundation type and to estimate foundation depths. The Pinellas Trail Bridge, currently under construction, is founded on 24-inch concrete piles. The piles were required to be driven within a preformed pile hole in order to achieve Minimum Pile Tip Elevations needed for lateral stability. Preforming was required due to the hard indurated clay and limestone encountered above the Minimum Penetration Elevation of the piles. We expect that the planned bridge may have a similar approach in the event that subsurface conditions of the planned bridge are similar to the Trail bridge. In addition, the bridge foundations for the water crossing will be exposed to scour and therefore, scour will be considered in the foundation design.

Tierra owns and operates one of the largest and most diverse fleets of drilling equipment in Florida. Tierra's equipment includes truck mounted, track mounted and amphibious drill rigs, as well as a barge-mounted drill rig (image below) which is ideal for the proposed bridge over the Lake Tarpon Outfall Canal.



Tierra performing bridge boring within the Hillsborough River for the widening of the Selmon Expressway

Geotechnical Approach

The geotechnical approach for design will include reviewing published data including the USDA NRCS Soil Survey information and USGS topographic maps in addition to plans and construction records available for the existing bridges at the site. Tierra will develop a subsurface investigation plan based on the review of available data, existing site conditions and coordination with the design team regarding proposed roadway improvements and the new bridge structure.

A "Call Sunshine" utility clearance will be completed subsequent to staking the test boring locations in the field. Drilling will not commence until all utilities are noted as "cleared" of conflict. Upon completion of the field work, Tierra will visually classify soil samples using the Unified Soil Classification System (USCS) and American Association on State Highways and Transportation Officials (AASHTO) soil classification systems, as appropriate for the project. Laboratory testing will be performed on select soil samples to confirm the visual classifications and establish environmental classifications for the bridge location.

The results of the geotechnical exploration program will be presented on plan sheets that characterize the subsurface conditions encountered at each test boring location. The soil types encountered and corresponding laboratory test results will be summarized on a Soil Survey sheet and will include recommendations regarding the use of the subsurface materials encountered. Tierra will coordinate with the design team on the proposed foundation types for the bridge structure and present geotechnical evaluations and recommendations for the selected foundation type. Soil conditions present at the site will be incorporated into the analyses with the goal of producing cost-effective designs.

Geotechnical reports will be prepared that summarize the subsurface conditions and results of the review of existing bridge data and the results of our field exploration and laboratory testing. Geotechnical recommendations will be provided to guide design and construction of the project.

TCP and Construction Phasing

Temporary traffic control for vehicular traffic will play a major role during the alternatives analysis and design phase of the project. KCA will develop a temporary traffic control plan for the preferred alternative that has the least impact on travelers.

If the proposed bridge is designed in the same footprint of the existing structure, phased demolition of the existing two lane bridge may be possible to allow a single lane of traffic on the bridge. This phased approach could cause queuing back-up onto

the mainline of NB McMullen Booth Road during peak hours for all NB exiting traffic. KCA has performed an initial traffic investigation to determine that providing a single lane ramp allowing right turn only at the Tampa Road intersection is possible without queuing traffic onto the mainline. Advanced signage and stakeholder education/outreach prior to construction would be needed for eastbound only exiting traffic and other traffic would be required to continue NB and detour at the East Lake Road Business exit as shown in the graphic below.

As discussed previously, Alternative 2 would allow the existing bridge to maintain 2 lanes of traffic during phased construction of the proposed bridge causing the least impact to travelers during construction.

Public Involvement

The County places high priority on public involvement and KCA will provide the tools and support to engage stakeholders early in the design process. As part of the East Lake Road Corridor Analysis Study, the County hosted virtual and in-person project information meetings to share corridor wide improvement alternatives and gain stakeholder feedback. Although not specific to the off-ramp bridge replacement, KCA will use applicable feedback from these meetings to tailor an outreach plan to present the selected bridge replacement alternative and effects on the community and users during and after construction. KCA is also aware of local groups within the East Lake community having special interests in projects corridor wide such as the Council of North County Neighborhoods (CNCN). KCA's team will be available for small group meetings or periodic updates to the Board of County Commissioners, CNCN and other groups in conjunction with the other projects within the corridor. Since there will be several ongoing design projects along with the corridor wide PD&E Study, KCA's public involvement experts will be in coordination with the County and all teams to effectively provide information to the



Possible detour during construction of the proposed bridge

community and stakeholders utilizing the County's East Lake Road Corridor webpage, social media, project information meetings, and other means of communication.



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Project Management Approach

The KCA team is committed to delivering a successful project to Pinellas County. Ms. Lawson will be the key contact person for the County on both technical and administrative issues. It will be her responsibility to know the status of each design discipline at all times and relay any information to the County's PM, Arturo Martinez, PE. Ms. Lawson's prior position in the Transportation Engineering Section of Pinellas County Public Works will also give her insight on the support and information to have readily available regarding the project. She will accomplish this through frequent and direct contact with the EORs for each discipline. Ms. Lawson will maintain control of the project by monitoring the schedule and technical issues and by effective communication/coordination between the County, internal design team, and subconsultants. Regular project production meetings will be held with all discipline leads starting immediately after the County issues Notice To Proceed (NTP). This approach has proven successful on previous similar projects.

Tampa Road Intersection Graphic from East Lake Road Corridor Public Meetings

The KCA team believes there is no greater testimony to the quality of our team's work than the fact that our clients continue to select us to work on their most critical infrastructure. As a partner for Pinellas County for over 35 years, we understand the commitment to delivering a successful project every time.

Fundamental project processes include:

- Development of a project plan at the outset of the contract that identifies the EORs and key personnel for each discipline, schedule, and scope of services
- Distribution of incoming correspondence to all team members assigned to the project, ensuring each individual is aware of any issues
- Submittals to the County and others, such as utility owners, with copies of documents forwarded to the lead engineers
- Biweekly internal progress meetings for all EORs in each discipline
- ➤ Monthly progress meetings with the County, if requested, with attendance by the EORs and Ms. Lawson

- ➤ Attendance at Pinellas County BOCC meetings as required
- ➤ Monthly progress reports and schedule updates
- Documentation of phone conversations concerning project issues
- Scheduling and moderation of team meetings to coordinate issues between the various engineers and disciplines involved
- ➤ Extensive and continuous use of email, internet, SharePoint, OneDrive, and Teams site to coordinate issues and transfer files between the County and team members

KCA will work closely with the County and keep the PM and other key staff informed of all critical issues. Plans will be submitted to the County for review and comments at established project milestones, and we will respond to any comments within two weeks of receipt. A review meeting to discuss comments requiring further coordination will be held with the County, Ms. Lawson and EORs. Additionally, KCA maintains relationships with Pinellas County, FDOT, Southwest Water Management District, USACE, and FDEP. These well-developed relationships will be invaluable in coordinating issues between the County and permitting agencies. Continued attention to the project will be ongoing through post design. Ms. Lawson and the KCA team will provide assistance during the advertisement and bidding process by responding to Contractor questions and reviewing bids the County receives. During construction, KCA will take part in construction meetings, field reviews, and provide timely reviews and responses on shop drawings and requests for information (RFI).

KCA team members are ready to assist the County in any request associated with this project. Our PM and key technical staff are always available to address any questions or attend meetings with the County. Part of our commitment to the County is to provide the assurance that a responsible PM and Deputy PM are always available. Stephanie Boyd, PE, Deputy PM, is the secondary point of contact for the project so the KCA team can commit 100% availability for the project.

Ms. Lawson and Ms. Boyd will develop a monthly production report that is tailored to the needs of the County's PM. This report will provide a concise account of the work that was performed during the period and an account of the progress of the specialty subconsultants directly corresponding to monthly invoices. The production report will also provide the County PM a one-month look ahead of the various activities planned and will allow Ms. Lawson to accurately assign and control staffing levels. One tool KCA uses to manage projects in an efficient manner is internal coordination meetings. Another tool Ms. Lawson utilizes is Power BI where she will track the staff utilization versus the project's budget in real time. Every week KCA's PMs discuss staff assignments with our team and subconsultants working on the project.

We will also conduct phase review meetings with County staff to ensure that all questions and comments are addressed prior to the next phase submittal.

WILLINGNESS AND ABILITY TO MEET SCHEDULE AND BUDGET

KCA recognizes the importance of keeping this project on schedule to maintain the County's CIP budget and complete construction by year 2030 as stated in the RFP. We proposed a 24-month design schedule. This schedule will allow sufficient float time for County review of the Alternatives Analysis Report and alignment selection, acquire surveys, phase plan submittals, permitting, utility coordination, and R/W acquisition (if required). After the County issues notice to proceed (NTP), a critical path method (CPM) project schedule will be prepared in Microsoft Project using the County's template and will be provided to the County with a breakdown highlighting major milestones and all tasks to be performed. The following major submittals will be included allowing 21 calendars days for County review of each:

- Draft Alternatives Analysis Report and Pond Siting Reports
- Final Alternatives Analysis Report and Pond Siting Reports
- Bridge Development and Hydraulic Reports
- > 30% Complete Plans Submittal
- 60% Complete Plans Submittal
- > 90% Complete Plans Submittal
- Final Plans Submittal

All major submittals will include an Engineer's Estimate of Probable Construction cost. We will utilize recent County bid tabs along with statewide average unit costs to develop the estimate which will be reviewed and updated at every milestone to ensure a high level of accuracy. KCA understands the importance of these estimates to ensure that the project can be completed as planned by the County's CIP.

Perhaps the most important aspect of KCA's project management approach is our PM's awareness of the schedule. Managing all tasks from kickoff to production, to permitting, to acquisition and construction bid documents, and ensuring everything in between occurs in the correct order to ensure the project stays on schedule. A project that has a sound technical approach from the very beginning will flow smoothly through critical path items because all important decisions will have been made at the correct time. Conversely, any component of



the project that is not completed in the proper sequence could jeopardize the schedule at a later sequence. Ms. Lawson, our PM for this project, has more than 20 years of engineering and management experience and is well versed in recognizing potential problems that may impact the schedule and will address them immediately. Proper coordination with regulatory agencies and UAOs will also alleviate potential delays to the schedule. Managing subconsultants and ensuring they understand the project scope and schedule is vital. Our team members must realistically forecast their upcoming workload to ensure they will have the available manpower to perform the tasks when the schedule dictates. To help manage all of this, we use Microsoft Project scheduling software and project management tools including Microsoft SharePoint. We utilize SharePoint as a collaborative platform to store and share documents with all team members and to facilitate effective team communications. Regular tracking of work efforts completed, and time expended provide the County's PM with the best insight on a project's progress. Ms. Lawson will be involved in every aspect of the project to ensure the work plan is effectively executed. Her emphasis on communication and keeping Pinellas County's PM informed at all times through timely meeting minutes, progress reports, and project review updates will ensure there are no surprises. KCA utilizes Deltek Vantagepoint as our accounting project management database and Microsoft's cloud-based Power BI (Business Intelligence) platform to monitor the work efforts and associated fees expended on each project. The negotiated fee/hours will be used to develop the appropriate tasks and subtasks within our project management database, which will permit the constant comparison between project completion and payout.

KCA's Power BI platform can produce very detailed reports for our PM such as:

- Total time spent on the project
- Time spent on specific tasks
- Detailed project budgetary data
- Status of Invoicing
- Milestone tracking

Schedules for all projects are maintained on a central board that can be reviewed by staff for conflicts and peak production times. During our weekly production meetings, upcoming submittal dates are discussed and coordinated with production personnel to meet our schedule milestones. Reports of work efforts to date versus the projected efforts based on the critical path method schedule are also used by KCA's managers to review progress.

All of KCA's electronic project files are backed up and secured "in the cloud" and in-house software support is available for all projects, providing additional reassurance to the County that schedules will never be impacted by lost project data.



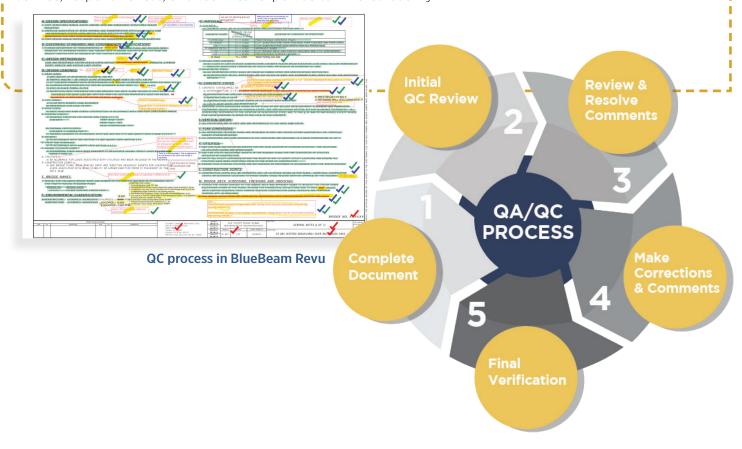
Engineer's Opinion of Probable Construction Cost



QA/QC

Pinellas County expects delivery of final work products and services to be thoroughly reviewed by experienced professionals. They also expect projects to be of high quality, completed on time, and within budget. The KCA team is committed to exceeding these expectations. A key component of our approach is an effective quality assurance/quality control (QA/QC) program, which is as important as the design and plans production process. Although our design professionals follow stringent internal QC processes, we believe it is critical to have senior professionals in each design discipline perform independent peer reviews to ensure delivery of a quality product. Peer reviews focus not only on the technical design standards and project scope requirements, but also on conflicts, cost, and constructability.

Providing quality services and products on time and within budget is the sure way to achieve client satisfaction. QA/QC is part of the KCA culture. We have developed a rigorous review process that is used on every project during the design and production process. We use a five-step color coded checking system, utilizing Bluebeam for all plans, reports, design calculations, documentation, and deliverables. This is a digital QC collaboration tool that helps the reviewers from different disciplines to work in one PDF file at the same time and all reviewers can see the comments that are made in real time. This helps to manage, measure, and control the review process to eliminate errors, omissions, repeat and conflicting comments, while maintaining high quality design standards. The final QC document is a clean legible document with comments from all reviewers in one PDF file. This file can be provided to the County upon request to ensure the QC process is being followed. All project deliverables are peer reviewed by staff members who have not been directly involved in development of the project. Prior to implementation of the project-specific QA/QC plan, the document will be submitted to the PM, Ms. Lawson, for approval. Ms. Lawson will also verify our proposed subconsultants implement a comparable QA/QC plan, also minimizing mistakes and impacts to the project schedule. In addition to KCA's standard checks and balances, the County's Design Submittal Sufficiency Checklist will be completed for each phase submittal and certified by both KCA's PM, Ms. Lawson, and firm principal, **Guillermo Madriz**, **PE**. The Design Submittal Sufficiency Checklist will be part of the deliverables to the County and included in our project specific QA/QC plan. KCA's QA Manager, Richard Harrison, PE, will verify our QA/QC plan is followed by all team members. We are dedicated to delivering the highest level of service, responsiveness, and technical expertise to Pinellas County.



WORKLOAD AND AVAILABILITY

The KCA team has been carefully structured to provide exceptional service and experienced staff in the performance of this important County project. We commit that the key team members, identified on our organizational chart, will be available for the work anticipated under this project. We also realize flexibility throughout a project is important. Flexibility is essential when unforeseen circumstances occur. For this reason, we have backup staff available for key KCA team staffing positions to ensure that County expectations are met without sacrificing schedule. One of the most important considerations in evaluating workload and ability of the KCA team to handle the scope of services is comparing the specific expertise needs to that of the KCA team. The key staff identified on the organizational chart are immediately available to work on this project. We have studied our current and future workload and many of our current projects are nearing completion, freeing up staff to work on this contract. We have reviewed the staffing availability over the next 18 months for our key team members and determined they will all have adequate time to prioritize this project. The charts below show our committed workload and availability for this project and the availability of the key staff identified respectively.

60%	Kellie Loper, PE, Constructability	25%
65%	Tom Pride, Environmental/Permitting	25%
40%	Joseph Antinori, PE, Geotechnical [Tierra]	40%
65%	Sandra Polanis, Geotechnical/ Contamination [DPS]	40%
40%	Pat McCormack, PSM [C&F]	40%
60%	Kenneth Hardin, Cultural Resources [Janus]	85%
	Kelly Joslin, R/W Acquisition [IAA]	65%
	65% 40% 65%	Tom Pride, Environmental/Permitting Joseph Antinori, PE, Geotechnical [Tierra] Sandra Polanis, Geotechnical/ Contamination [DPS] Pat McCormack, PSM [C&F] Kenneth Hardin, Cultural Resources [Janus]

SBE AND MBE UTILIZATION

KCA strives to build long-term relationships with SBE and MBE firms through our work in the engineering industry. We make every effort to meet or exceed established SBE and MBE usage goals on each project we undertake. C&F, DPS, and IAA are Pinellas County-certified SBE firms while DPS, IAA, and Tierra are State of Florida-certified MBE firms. Their SBE and MBE certificates are included in Tab 2: Statements and Documentation.

Not only is KCA committed to the utilization of W/M/DBE firms, but we are also a certified MBE with the City of Tampa.

SERVICE FACILITIES AND EQUIPMENT

KCA has provided engineering, planning, permitting, and inspection services to the transportation

industry for 48 years. Our offices have the equipment necessary to facilitate a prompt service response time. KCA maintains a list of equipment readily available to successfully perform the required services for this study.

KCA provides our employees with the latest hardware, software, and training. Our engineers and technicians have the latest versions of AutoCAD Civil 3D and Bentley MicroStation. Microsoft Teams is used to facilitate project communication and update the entire team on project status at a moments notice. KCA staff will utilize AutoCAD Civil 3D and the Pinellas County Kit in accordance with the County's Civil 3D CADD Manual to design and produce contract plans for this project.

We encourage all employees—whether they are a PE, CADD technician, or accountant—to join their respective trade organizations and professional societies so they can stay abreast of the latest technical information in their fields. Our CADD technicians and engineers are all knowledgeable and proficient in our client's CADD standards and practices including the County's standards.

OFFICE LOCATIONS

KCA corporate/primary office, located at 201 N Franklin Street, Suite 900, Tampa, Florida 33602, will be the responsible office for this project. The office is in the heart of downtown Tampa with easy access to I-275, I-4, and the Selmon Expressway, as well as 30 minutes from the County's office. Our Tampa office employs more than 120 professionals including the majority of our proposed project team for this project. Majority of KCA work efforts will be performed in Tampa, which will facilitate frequent face-to-face meetings with County staff, regular oversight of project activities through frequent site visits, and immediate response for situations that may require rapid response times. These offices embody KCA's long-standing commitment to the Tampa, St. Petersburg, and Clearwater area, and makes it possible for us to channel our broad technical resources to the County.

Our extensive resources will ensure the necessary level of professional involvement for the responsible, expeditious, and accurate completion of this project. KCA also has offices throughout Florida including Fort Myers, Orlando, Tallahassee, Miami, Immokalee, Gainesville, and Fort Lauderdale.



All KCA key team members are in close proximity of each other and Pinellas County. Our team is familiar and knowledgeable of County policies, procedures, staff, and goals for this project; therefore, they are personally invested in the successful completion of this project.

SIMILAR PROJECT EXPERIENCE

KCA regularly provides engineering services to a wide variety of clients, including county governments, local municipalities, state departments of transportation, and other government organizations. Our professionals routinely serve as extensions of local government staff, working in the agencies' offices and providing on-site assistance.

Below is an overview of additional relevant project experience. The projects to be assessed as part of this proposal's evaluation can be found in the SF 330 on Tab 2.

CITY OF CLEARWATER EOR, CLEARWATER, FL

KCA has completed several projects as an existing EOR for the City of Clearwater including inspection of the SR Pier 60, pavement evaluation and five-year improvement plan for City-owned and maintained roads, forensic evaluations of failed pavement on Hercules Avenue, and first comprehensive inventory of sidewalks within the City. Under the City's EOR contract, KCA was tasked with providing engineering services to replace four bridges—Bridge No. 155513: Island Way NB over Clearwater Harbor; Bridge No. 155514: Island Way SB over Clearwater Harbor; Bridge No. 155515: Harbor Passage West Bridge over Clearwater Harbor; and Bridge No. 155516: Harbor Passage East Bridge over Clearwater Harbor—near Clearwater Beach. These phase



Island Estates Bridge: after replacements

constructed bridges were heavily coordinated with the City and surrounding communities. Services consisted of structural engineering, roadway design, environmental, permitting, drainage, survey, and coastal engineering. KCA also provided structural engineering services to repair the seawall as well as utility coordination, TCPs, bridge inspection, and load ratings.

FRED HOWARD PARK BRIDGE REPLACEMENT, PINELLAS COUNTY

Fred Howard Park consists of 155 acres and is located on the Gulf of Mexico. Nearly two million visitors enjoy the park and its facilities annually. In addition to its many amenities, Howard Park's location provides access to the Gulf of Mexico by a one-mile long causeway. Due to the extremely aggressive environment and heavy usage, these bridges became severely deteriorated and no longer met the needs of the park visitors. Pinellas County decided to replace the bridges with the goal of providing a structure with design life of 75 years and increased safety for pedestrian traffic such as bicyclists, walkers, and fishermen.

Park visitor and worker safety and construction duration was extremely important to the County. After much consideration, it was decided to close the beach area and causeway during construction. This approach would eliminate phase construction and worker/visitor interaction, resulting in a safer, quicker, and more cost-effective project. Advanced signage and public notification was utilized to inform the public of the temporary closure. The original structures were flat slab bridges comprised of four 30-foot spans that accommodated one lane of traffic in each direction, with minimal shoulders and sidewalks. The bridges also carried water, sewer, telephone, and electrical utilities for the facilities provided at the beach area. The new structure consists of prestressed slab units with a 6½-inch thick cast-in-place topping spanning three 37-foot-6-inch spans. The revised span lengths avoid conflicts between the existing and new substructure units. The new bridges still accommodate one lane of traffic in each direction; however, 4-foot minimum shoulders were provided for bicyclists, and the new 8-foot sidewalks are protected with traffic railing barrier. The existing utilities were



Fred Howard Park Bridge Replacement

incorporated into the new bridges as well. This project won the 2010 APWA Public Works Project of the Year.

STUDIES AND IMPROVEMENTS ALONG THE CR 611 CORRIDOR, PINELLAS COUNTY, FL

Under our Pinellas County Miscellaneous Services contract, KCA has conducted various studies and infrastructure improvements along the CR 611 corridor in Pinellas County. Our work includes a Preliminary Engineering Report (PER) for pedestrian enhancements at the CR 611 and CR 752 corridor, evaluating multiple alignment alternatives to connect the Pinellas Trail across the intersection of CR 611, CR 752, and the Lake Tarpon Outfall Canal.

Additionally, KCA has prepared a PER for the replacement of the northbound off-ramp bridge over the Lake Tarpon Outfall on McMullen Booth Road (CR 611). As part of the Ream Wilson/Pinellas Trail improvements at Old Coachman Road, KCA will provide design and



Aerial Drone Images of Existing Corridor

permitting services for a 500-foot, ADA-compliant trail, including the replacement of a pedestrian bridge spanning Alligator Creek.

BIMINI DRIVE BRIDGE REPLACEMENT | HARBOUR DRIVE BRIDGE REPLACEMENT, MONROE COUNTY, FL

KCA is providing structures, drainage, and roadway design; environmental assessment and permitting; utility coordination; and public involvement services for the Bimini Drive Bridge (No. 904603) Replacement and Harbour Drive Bridge (No. 904604) Replacement projects. These bridges are two of four historic Duck Key bridges, requiring evaluation under NHPA. KCA led the Section 106 (NHPA) process to a successful conclusion, obtaining a MOA for all four historic bridges. Tasks include assessment of wetland, seagrass, coral, and protected species impacts resulting from the proposed structure replacement as well as coordination with federal and state resource and regulatory agencies.



Bimini Drive Completed Bridge Replacement

Additional tasks include obtaining USACE 404 dredge and fill permit, USCG Bridge Permit, FDEP NPDES permit, and SFWMD ERP.

These bridges provide the only access to the Island of Duck Key. As a result, the bridge replacements included a phased construction sequence to maintain resident access and utility operation throughout all phases of construction. Innovative solutions were implemented to minimize noise and vibrations in this residential neighborhood.

VENDOR SUBMITTAL ACKNOWLEDGEMENT FORM

It is the policy of Pinellas County, Board of County Commissioners, to accept the lowest responsive and responsible or highest ranked submittal received meeting specifications. No changes requested by a vendor due to an error in pricing will be considered after the advertised solicitation opening date. By signing this Vendor Submittal Acknowledgment Form, vendors are attesting to their awareness and acceptance of this policy and agreeing to all solicitation of terms and conditions, including any insurance requirements.

Vendor Name (as shown on W-9): Kisinger Campo & Associates, Corp.

Doing Business As (DBA) (if applicable): n/a

Mailing Address (as shown on W-9): 201 N. Franklin Street, Suite 900

City, State, Zip (as shown on W-9): Tampa, FL 33602

Vendor Email (primary company email): KCA-Marketing@kcaeng.com

Remit to address (as shown on vendor invoice): 201 N. Franklin Street, Suite 900, Tampa, FL 33602

Federal Tax ID (FEIN) #: 59-1677145

Vendor Contact Information

Contact Name: Erin Lawson, PE

Phone Number: 813.871.5331

Email Address: KCA-Marketing@kcaeng.com

Payment Terms: Net 45 (per Florida Statute F.S. 218.73) N/A % N/A Days

Deposit (if required) has been paid in the amount of \$ N/A

Proper Corporate Identity is needed for a firm registered with the Florida Division of Corporations. Please visit dos.myflorida.com/sunbiz/ for this information. It is essential to return a copy of your W-9 with your submittal.

I hereby agree to abide by all terms and conditions of this solicitation, including all insurance requirements, and certify that I am authorized to sign this solicitation for the vendor.

Authorized Signature:

Print Name: Guillermo Madriz, PE

Title: Vice President/Director of Corporate Operations

THIS FORM MUST BE RETURNED WITH YOUR RESPONSE